



**Staff Report to the St. Petersburg Community Planning & Preservation Commission (CPPC)**

Prepared by the Planning & Development Services Department  
Urban Planning and Historic Preservation Division

For Public Hearing on Wednesday, February 14, 2023  
at 2:00 p.m. in the City Council Chambers, City Hall  
175 Fifth Street North, St. Petersburg, Florida.

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**City File: ZM-15**  
**NTM-1 Zoning Map Amendment**

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This is a city-initiated application requesting that the Community Planning & Preservation Commission (“CPPC”), in its capacity as the Land Development Regulation Commission (“LDRC”), make a finding of consistency with the City of St. Petersburg’s Comprehensive Plan and recommend to City Council APPROVAL the following proposed map amendments to the *Official Zoning Map* from NT-1 and NT-2 (Neighborhood Traditional) to NTM-1 (Neighborhood Traditional Mixed Residential) for approximately 2,897 properties.

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**APPLICANT INFORMATION**

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**APPLICANT:** City of St. Petersburg  
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St. Petersburg, Florida 33712

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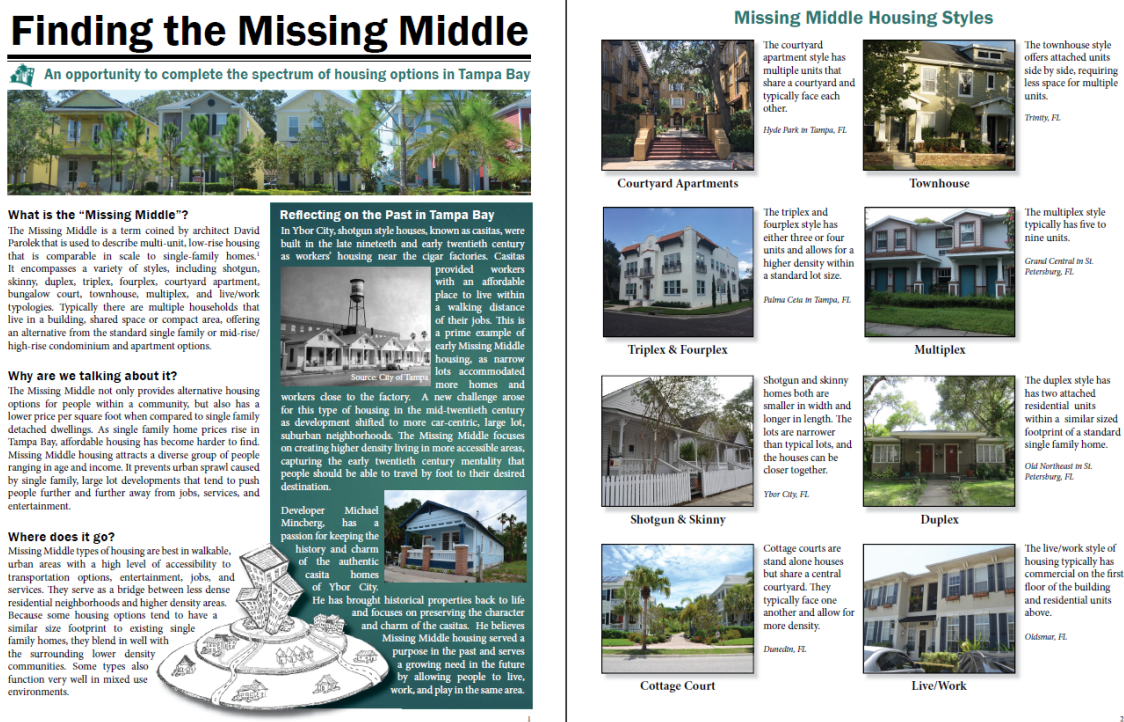
**INTRODUCTION: HOUSING DEVELOPMENT**

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This city-initiated application is a proposed amendment to the City of St. Petersburg’s *Official Zoning Map* and represents the convergence of several different housing initiatives.

In 2017, the City’s Planning and Development Services Department began investigating the potential for accessory dwelling units and small-scale, multi-family developments to increase housing diversity and housing supply while mitigating for an increase in population, housing demand, and price inflation. This research helped inform a publication in October 2017 by Forward Pinellas, a countywide planning agency, titled “Knowledge Exchange Series: Finding the Missing Middle.” The term Missing Middle housing generally refers to multi-unit or clustered housing that is compatible in scale and design with single-family houses, and is designed to encourage walking, biking, and transit use.

Figure 1. Knowledge Exchange Series: Finding the Missing Middle, published by Forward Pinellas, October 2017



Following publication, Staff delivered a two-part presentation on housing development and affordability to the City Council’s Committee of the Whole (“COW”) on March 22, 2018, and April 19, 2018. The earliest concepts of the NTM-1 zoning category were introduced during the April 19<sup>th</sup> presentation.

Shortly afterwards, Staff began a six-part information series. The information series was hosted for the public at the then-named St. Petersburg’s Main Library and extended from June 26, 2018, to September 25, 2018. Discussion topics included:

- Density, building typologies, and the creation of one or more zoning categories to provide a variety of urban housing choices in medium-density building types including single-family houses, accessory dwelling units, duplexes, small multiplexes, bungalow courts (“tiny” houses), courtyard buildings, detached row houses (“skinny”), townhouses, and large multiplexes.
- Transportation initiatives and parking regulations, proximity to major streets, multi-modal transit options, activity centers, and community redevelopment areas.



- Affordability initiatives, funding mechanisms, housing assistance programs, affordable housing initiatives in the South St. Petersburg Community Redevelopment Area, and Penny for Pinellas affordable housing funding.
- Affordable and workforce housing density bonuses, development bonuses within the Downtown Center to prioritize affordable and workforce housing units and establishing additional activity centers throughout the City.

Public feedback during this information series had an influential impact on development of the NTM-1 zoning category and inspired other text amendments to the City Code regulating workforce housing and housing development bonuses. On December 12, 2019, City Council adopted Ordinance 405-H establishing the NTM-1 zoning category and codifying the applicability criteria used to identify the parcels included with this proposed map amendment. Originally scheduled for the Spring 2020, this proposed map amendment was eventually delayed due to the COVID-19 pandemic.

Concurrent with efforts to establish the NTM-1 zoning category, then-Mayor Rick Kriseman also announced plans to develop “StPete2050: A Vision Plan for St. Petersburg.” The purpose of the StPete2050 Plan was to perform a progress and opportunities inspection of the “Vision 2020 Plan,” adopted in 2001 and effectuated through city-wide zoning and comprehensive plan amendments in 2007. The StPete2050 Plan also included new feedback from a changing community about our citizen’s priorities and aspirations for St. Petersburg’s future. The StPete2050 Plan is organized around ten (10) community themes, including *Housing*. This theme addressed several needs, including the identification of Missing Middle housing as an emerging opportunity.

Also concurrent with these efforts with NTM-1 and the StPete2050 Plan, then-Mayor Rick Kriseman announced “St. Petersburg’s Housing Plan: For All, From All” in 2020, which similarly proposed development of accessory dwelling and other housing units within a traditional neighborhood context. This policy further reinforced the City’s commitment to improve housing diversity and supply, including use of the NTM-1 zoning category.

Following completion of the StPete2050 Plan in May 2021, Staff met with City Council on at least five (5) separate occasions from August 2021 through August 2022 to consider whether the applicability criteria identified in City Code Section 16.20.015.2 (adopted as Ordinance 405-H) should be expanded to include a larger cross-section of the City’s residential neighborhoods. Following the August 2022 meeting, Staff was directed to proceed this map amendment application using the applicability criteria adopted in 2019.

The proposed map amendment is intended to generate a variety of alternative housing options for varying economic levels in our community and provide more dwelling units in response to market demands of first-time home buyers, smaller families, couples, retirees looking to age in place, adults with disabilities, car-free households, and many others.

**ZM-15: PROPOSED NTM-1 ZONING MAP AMENDMENT**

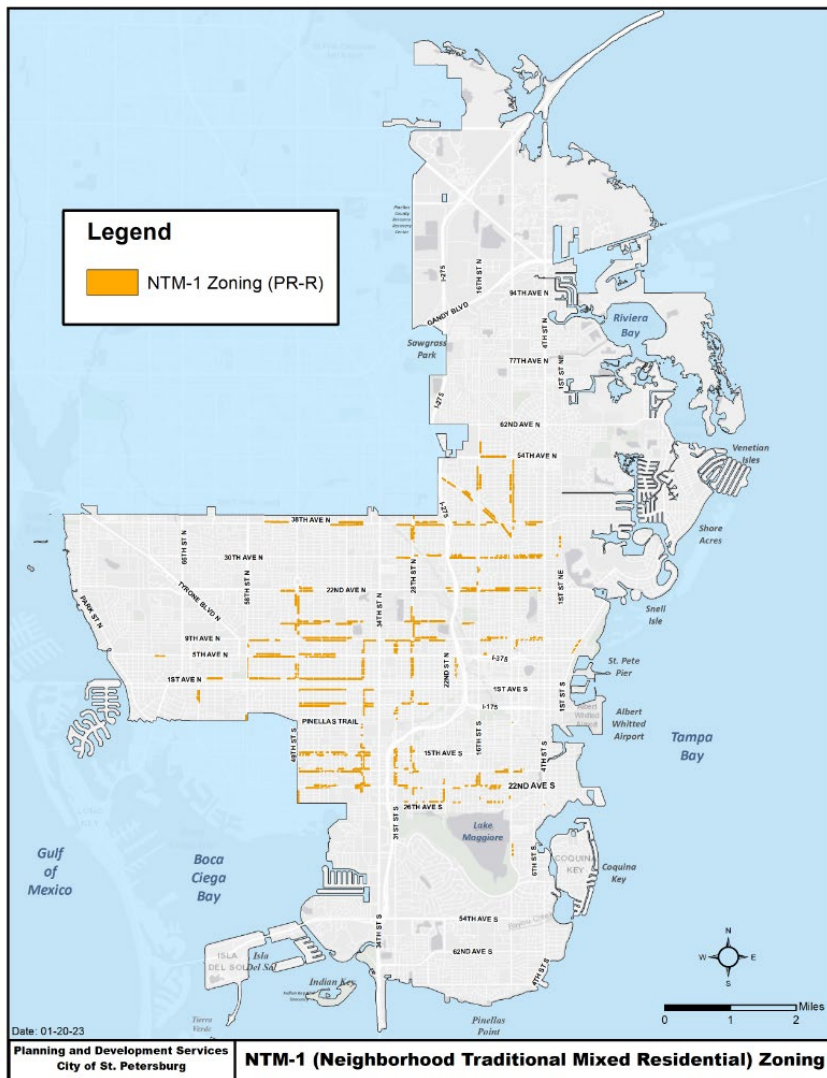
This is a city-initiated amendment to the *Official Zoning Map* from NT-1 and NT-2 (Neighborhood Traditional) to NTM-1 (Neighborhood Traditional Mixed Residential). Qualified parcels included within this application have an existing *Future Land Use Map* designation of PR-R (Planned Redevelopment – Residential) and meet the locational criteria set forth in City Code Section 16.020.015 including:

- 175-feet from the centerline of a designated Future Major Street;
- retains direct connectivity to one or more adjoining Future Major Streets; and
- adjacent to a public alley;
- *a minimum of 75% of the property\** is outside of the designated Coastal High Hazard Area ("CHHA") and does not increase density in CHHA.

*\*As proposed by LDR 2023-01*

The purpose of this proposed map amendment is to assign the NTM-1 zoning category on the *Official Zoning Map* in accordance with the applicability criteria in City Code Section 16.20.015.2 (adopted as Ordinance 405-H) and as intended upon establishment of the zoning category in 2019. The proposed zoning category is consistent with the PR-R Future Land Use designation; therefore, a *Future Land Use Map* amendment is not required. If approved, this amendment will qualify approximately 2,897 parcels located within an area that is covering 75 neighborhood and civic/business associations (See Figure 1).

Map 1. Proposed NTM properties. Additional maps included in the report attachments.



The purpose of the NTM-1 zoning category is to provide a variety of urban housing choices in low to medium density building types that reinforce the walkability of the neighborhood, provide a variety of attainable housing choices, establish appropriate transition zones from mixed-use corridors to single-family housing, support neighborhood-serving retail and service uses adjacent to this zoning category, and support public transportation and other multi-modal alternatives.

The NTM-1 zoning category is regulated through City Code Section 16.20.015, including lot dimensions, residential density, building and site layout and orientation standards. These development standards provide important context when considering any changes that might extend from this requested zoning map amendment. Originally adopted in 2019, the NTM-1 zoning category allows up to four (4) units on a typical lot not to exceed a maximum residential density of 30 units per acre; the existing NT-1 and NT-2 zoning categories allow up to 15 units per acre. This increased density enables a creative combination of housing typologies. For example, a 2-unit lot might have a duplex, two houses, or one house and an ADU; a 3-unit lot might have a triplex, or one house and two ADUs; or a 4-unit lot might have a fourplex or duplex and two ADUs.

During the public workshop series associated with this application, Staff received thoughtful feedback, insightful questions, and real scenarios to test the existing development standards. This feedback led to a text amendment application being processed as LDR 2023-01. On February 1, 2023, the City’s Development Review Commission (“DRC”) conducted a public hearing and made a finding of consistency with the Comprehensive Plan. The text amendment application will be considered by the City Council concurrent with this map amendment application in March 2023.

As part of the concurrent text amendment application, Staff is proposing for properties individually listed or located within a historic district in the National Register of Historic Places or St. Petersburg Register of Historic Places, that additional units are allowed when adaptively established within the existing principal structure or within an addition or accessory building when designed subordinate to the principal structure. For development of vacant lots or redevelopment involving demolition, building setbacks and floor area ratio is proposed to be consistent with the current NT standards, providing for compatibility with the established development pattern in these districts:

Table 1. Comparison of Development Potential: NT to NTM-1

	Existing NT	Proposed NTM-1	Historic Properties *
<b>Density (units/acre)</b>	15	30	
<b>Impervious Surface Ratio</b>	65%	75%	
<b>Lot Area, Minimum</b>	4,500 s.f.	2,000 s.f.	
<b>Lot Width, Minimum</b>	45 ft	20 ft	
<b>Building Setbacks:</b>			
<b>Front yard</b>	25 ft	18 ft	25 ft
<b>Street Side</b>	12 ft	8 ft	12 ft
<b>Interior Side</b>	5-6 ft	3 ft	5 ft

<b>Rear</b>	6-10 ft	22 ft (inc. alley width)	
<b>Floor Area Ratio (FAR)</b>	0.4-0.5 w/0.2 bonus	0.5 w/0.2 bonus	0.4 w/0.2 bonus
<b>Building Height</b>	24 ft to roofline 36 ft to roof peak	24 ft to roofline 36 ft to roof peak	
* Subject to concurrent approval of application LDR 2023-01.			

**RELEVANT CONSIDERATIONS**

The Urban Planning & Historic Preservation Division staff reviewed this application in the context of the following criteria excerpted from the City Code *Section 16.70.040.1.1 Amendments to the Comprehensive Plan and Land Development Regulations*, the review and decision shall be guided by the following factors:

**1. Compliance of the proposed use with the goals, objectives, policies, and guidelines of the Comprehensive Plan.**

The following staff analysis is provided to address compliance with the Comprehensive Plan’s Goals, Objectives, and Policies:

StPete2050 VISION  
Expand housing development/supply opportunities.

Goal *The NTM-1 map amendments are expanding the potential housing supply by amending approximately 2,897 properties that meet the qualifying guideline for the NTM-1 district. The existing NT zoning categories allow for one (1) principal unit, plus one (1) accessory dwelling unit (calculated at 15 units per acre). The NTM-1 zoning category allows up to four (4) units per lot (calculated at 30 units per acre) on a typical-sized 50-ft. x 127-ft. parcel.*

StPete2050 VISION  
Make attainable housing options, including rental and ownership options and a variety of housing types, available in all neighborhoods throughout the city.

Goal *This is a city-wide rezoning following the qualifying criteria previously adopted in 2019. The proposed map amendment is intended to generate a variety of more dwelling units in response to market demands of first-time home buyers, smaller families, couples, retirees looking to age in place, adults with disabilities, car-free households, and many others. The NTM-1 amendment meets this Vision Goal.*

LU3.6 Land use planning decisions shall weigh heavily on the established character of predominately developed areas where changes of use or intensity of development are contemplated.

*The NTM-1 development standards are intended to maintain neighborhood compatibility in building placement, scale, and design. When applied to the Official Zoning Map, the qualified properties are located along Future Major Streets. These streets have more intensive traffic than the internal neighborhood streets. In addition, many of these streets also include the City’s*



*transit routes, which support higher densities by providing multi-modal options and other transportation alternatives.*

- LU3.11 More dense residential uses (more than 7.5 units per acre) may be located along (1) passenger rail lines and designated major streets or (2) in close proximity to activity centers where compatible.

*The NTM-1 district requires that a property be located 175-feet from the centerline of a designated Future Major Street and retain direct connectivity to one or more adjoining Future Major Streets. All properties proposed for the NTM-1 map amendment regulations are consistent with this policy.*

- LU3.14 The conversion of single-family structures into multifamily units shall be in accordance with the LDRs, however, any associated variances will be discouraged.

*The zoning amendment to NTM-1 must follow all locational and design criteria as provided in Section 16.20.015 Neighborhood Traditional Mixed Residential (NTM).*

- LU3.15 The Land Use Plan shall provide housing opportunity for a variety of households of various age, sex, race and income by providing a diversity of zoning categories with a range of densities and lot requirements.

*The proposed map amendment expands use of Missing Middle housing which refers to multi-unit or clustered housing that is compatible in scale and design with single-family houses, and is designed to encourage walking, biking, and transit use. Developing Missing Middle housing increases the number of housing units while catering to a variety of demographics including millennials and multigenerational households that are looking for smaller homes in walkable neighborhoods.*

- LU4: The following future land use needs are identified by this Future Land Use Element: 1. Residential – the City shall provide opportunities for additional residential development where appropriate.

*The NTM-1 map amendment strengthens an existing ordinance allowing a diversity of housing typologies that are compatible with existing residential neighborhoods in the traditional context.*

- LU22.1 The City shall continue to pursue strategies which reduce GHG emissions and vehicle miles traveled.

*Providing NTM-1 zoning along transit routes, corridors, and Future Major Streets will potentially reduce GHG emissions as it provides alternative transportation to the residents of the neighborhood and allows for the walkability along major corridors to retail and services rather than driving.*

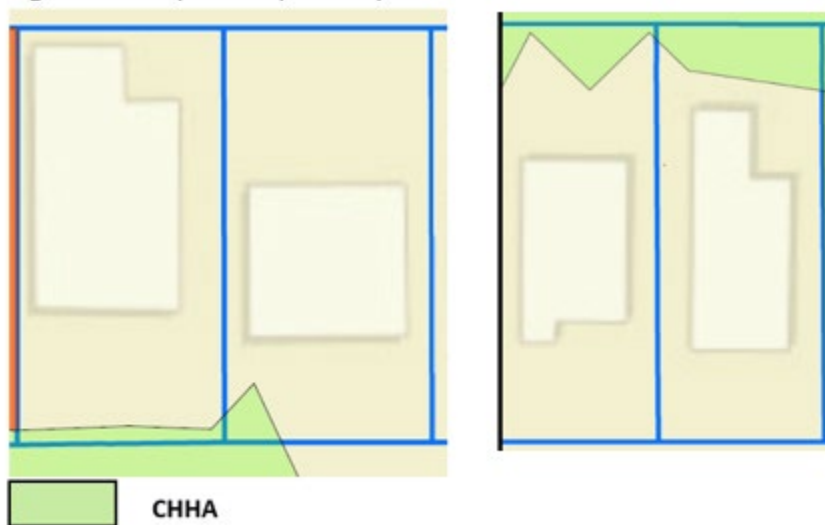
- LU23.3 The City's LDRs shall continue to support greater development intensity within the Corridor and Center zoning districts, particularly where located along fixed transit lines and around transit stops and stations.

*The proposed map amendments are located along the Future Major Streets and corridors where transit lines and stops are provided.*

CM10B The City shall direct population concentrations away from known or predicted coastal high hazard areas consistent with the goals, objectives, and policies of the Future Land Use Element.

*The map amendment proposes to include 27 parcels where 75-percent (%) or more of the property is located outside of the CHHA. This is proposed where the CHHA has a minimal effect on the property. Given that no increase in density will be allowed for that portion of the property in the CHHA, the text amendment is not considered a concentration of population within the CHHA. See Figure 1 for example properties where more than 75-percent (%) is outside the CHHA.*

Figure 2: Properties partially located in the CHHA; more than 75-percent (%) of the land is outside the CHHA.



T1.6 The City shall support high-density mixed-use developments and redevelopments in and adjacent to Activity Centers, redevelopment areas and locations that are supported by mass transit to reduce the number and length of automobile trips and encourage transit usage, bicycling and walking.

*The proposed amendment includes areas adjacent to Activity Centers, in redevelopment areas and supported by mass transit to reduce the number and length of automobile trips and encourage transit usage, bicycling and walking as provided in the Complete Streets program.*

**2. Whether the proposed amendment would adversely affect environmentally sensitive lands or properties which are documented as habitat for listed species as defined by the Conservation Element of the Comprehensive Plan.**

The proposed amendment does not adversely affect any environmentally sensitive land or properties which are documented as habitat for listed species as defined by the conservation element of the Comprehensive Plan. No P (Preservation) zoning category is proposed to be amended; and approximately 96-percent (%) of the subject parcels are already developed.

**3. Whether the proposed changes would alter the population density pattern and thereby adversely affect residential dwelling units.**

The proposed amendment properties have a Future Land Use designation of PR-R (Planned Redevelopment-Residential) allowing up to a maximum density of 30 units per acre when located outside the CHHA and when abutting a major street as depicted on the Future Major Streets Map (Map 20, Comprehensive Plan). From a Future Land Use determination, no changes are proposed to the population density pattern. The existing zoning designations of the NT districts (both NT-1 and NT-2) allow up to 15 units/acre. The subject amendment includes approximately 2,897 properties that are currently approved for 15 units/acre per the zoning district. Based on the zoning allowance there is a potential for the population to double in size.

St. Petersburg is currently growing at a Low-Medium rate according to the University of Florida Bureau of Economic and Business Research and based on this growth, it is projected that that the City will have a population increase of approximately 25,097 people over the next 30 years creating an annual demand for an additional 1,035 units per year.

The addition of 15 units per acre, assuming a density of 1.5 persons/household per multi-family unit (University of Florida Bureau of Economic and Business Research), an additional peak population of 8,715 people is estimated.

**4. Impact of the proposed amendment upon the adopted level of service (LOS) for public services and facilities including, but not limited to: water, sewer, sanitation, recreation and stormwater management and impact on LOS standards for traffic and mass transit. The POD may require the applicant to prepare and present with the application whatever studies are necessary to determine what effects the amendment will have on the LOS.**

The following LOS impact analysis concludes that the proposed rezoning will impact the City's adopted LOS standards for public services and facilities including potable water, sanitary sewer, solid waste, traffic, mass transit, recreation, and stormwater management; however, the City has adequate facilities to address the proposed zoning expansion. These LOS standards are updated annually through the *Concurrency Management Report* and related annual update to the Capital Improvements Element. Additionally, the property owner must comply with all laws and ordinances in effect at the time development permits are requested.

**POTABLE WATER**

Under the existing inter-local agreement with Tampa Bay Water (TBW), the region's local governments are required to project and submit, on or before February 1<sup>st</sup> of each year the anticipated water demand for the following year. TBW is contractually obligated to meet the City's and other member government's water supply needs:

- The City's adopted LOS standard is 125 gallons per capita per day (gpcd), while the actual current usage equates to approximately 72.4 gpcd. The proposed zoning projects a water use of 1.08 million gallons per day (mgd).

- The City’s overall potable water demand is approximately 26.8 mgd per day (mgd), while the systemwide capacity is 68 mgd. With only 39.4% of capacity systemwide currently being used, there is excess water capacity to serve the amendment area.

**SANITARY SEWER**

The city owns and operates the Water Reclamation Facilities (WRFs), each serves a distinct district that together comprises the St. Petersburg facilities planning area:

- The Northeast facility has an estimated excess average daily capacity of 8.18 mgd. The estimate is based on permit capacity of 16 mgd and a daily average flow of 7.82 mgd. With approximately 51.13% available capacity, there is excess average daily capacity to serve the amendment area.
- The Northwest facility has an estimated excess average daily capacity of 10.22 mgd. The estimate is based on permit capacity of 20 mgd and a daily average flow of 9.78 mgd. With approximately 51% available capacity, there is excess average daily capacity to serve the amendment area.
- The Southwest facility has an estimated excess average daily capacity of 5.26 mgd. The estimate is based on permit capacity of 20 mgd and a daily average flow of 14.74 mgd. With approximately 26.3% available capacity, there is excess average daily capacity to serve the amendment area.

With a projected sewer use of 1.5 mgd there is available capacity between the facilities to service the amendments. In addition, the Integrated Water Resources Master Plan incorporates growth projections and outlines the required system and network improvements.

Following several major rain events in 2015-2016, the City increased the system-wide peak wet weather wastewater treatment capacity from 112 mgd to approximately 157 mgd – a 40% increase in peak flow capacity. As outlined in the St. Pete Water Plan, the City is implementing system reliability improvements at the Water Reclamation Facilities (“WRFs”) aggressively improving the gravity collection system to decrease Inflow and Infiltration (“I&I”) which reduces peak flows at the WRFs, and addressing sea level rise system vulnerabilities. The City remains committed to spending approximately \$16 million a year in continued I&I reduction. Also, the City is fully committed to implementing selected recommendations from the St. Pete Water Plan, which incorporates growth projections and outlines the required system and network improvements needed to provide a resilient wastewater collection and treatment system.

**SOLID WASTE/SANITATION**

Solid waste collection is the responsibility of the City, while solid waste disposal is the responsibility of Pinellas County. The City and the County have the same designated LOS of 1.3 tons per person per year. The County currently receives and disposes of municipal solid waste generated throughout Pinellas County. All solid waste disposed of at Pinellas County Solid Waste is recycled, combusted, or buried at the Bridgeway Acres sanitary landfill. The City and County’s commitment to recycling and waste reduction programs have assisted in keeping down the actual demand for solid waste disposal, which continues to extend the life span of Bridgeway Acres Sanitary Landfill. The landfill is expected to remain in use for approximately 78 years, based on current design (grading) and disposal rates. Thus, there is excess solid waste capacity to serve the amendment area.



## **RECREATION**

The City's adopted LOS for recreation and open space is 9 acres/1,000 population, the actual LOS City-wide is estimated to be 20.8 acres/1,000 population. If approved, the city-wide estimate is 20.14 acres/1,000 population there will be no noticeable impact on the adopted LOS standard for recreation and open space.

## **STORMWATER MANAGEMENT/DRAINAGE**

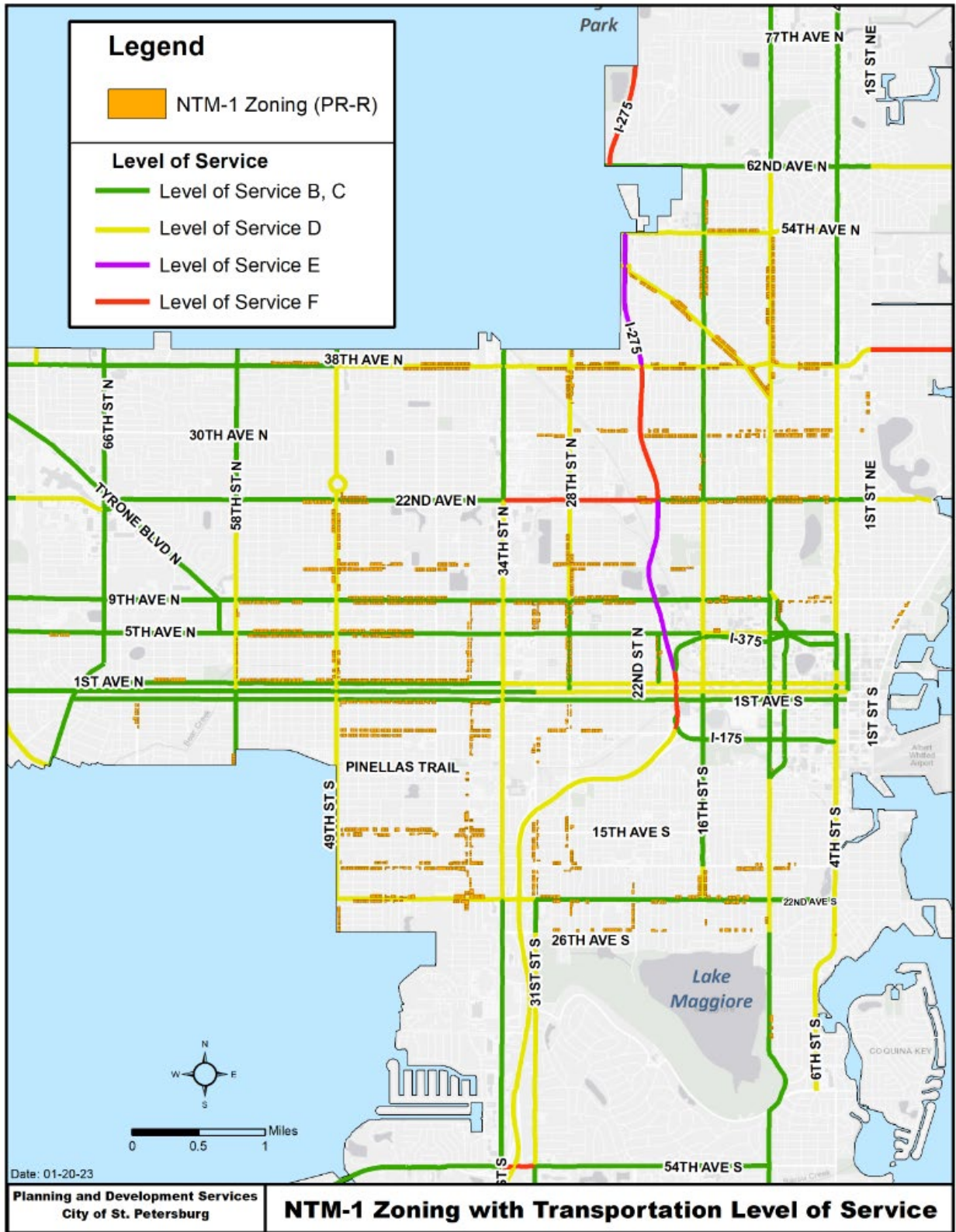
Stormwater management LOS is project dependent rather than based on the provision and use of public facilities and is not directly provided by the city for master planned developments. The LOS standard for drainage is implemented by the City through the review of drainage plans for new development and redevelopment where all new construction of and improvements to existing surface water management systems will be required to meet design standards outlined in the Drainage Ordinance, Section 16.40.030 of the Land Development Regulations. This ordinance requires all new development and redevelopment projects to be permitted through the City and SWFWMD to ensure projects meet quantity and quality design standards for stormwater treatment.

Prior to development of the properties with three (3) or more units, site plan approval will be required. At that time, City Code and Southwest Florida Water Management District (SWFWMD) site requirements for stormwater management criteria will be implemented. The City is currently updating its' Stormwater Master Plan as part of the One Water Plan. While this update is consistent with the SWFWMD guidelines, it is enhanced as it takes into consideration sea level rise to identify projects to maintain LOS and enhance water quality. The City's Stormwater Design Standards are being updated to incorporate Low Impact Design (LID) to reduce stormwater runoff and increase water quality. Likewise, the City recently updated its' impervious service mapping throughout the City and will be working towards a credit-based stormwater rate system for commercial and residential properties who implement LID and rain harvesting elements. Examples of such credits may be underground stormwater vaults, pervious pavements, greywater systems, and vegetative swales.

## **TRAFFIC**

Levels of service ("LOS") for roadway facilities adjacent to the parcels proposed to be rezoned to NTM-1 are shown on Map 6. According to the Florida Department of Transportation, roadway level of service is a quantitative stratification of a performance measure or measures that represent the quality of service, measured on an "A" to "F" scale, with LOS "A" representing the best operating conditions from the traveler's perspective and LOS "F" the worst. The Forward Pinellas' "2022 Annual Level of Service Report" is the source of LOS data for road segments on the maps that Forward Pinellas analyze. For road segments not analyzed by Forward Pinellas, City staff utilized LOS data from the "2008 Roadway Level of Service" from the City's Comprehensive Plan. All road segments function at a LOS "D" or better, except for 22<sup>nd</sup> Avenue North from I-275 to 34<sup>th</sup> Street, which functions at a LOS "F." LOS "D" is the City's former standard for capacity on its major road network when transportation concurrency was in effect. City staff submitted a grant application to Forward Pinellas' Multimodal Transportation Priority List in December for traffic signal improvements on 22<sup>nd</sup> Avenue North from 4<sup>th</sup> Street to 58<sup>th</sup> Street. These improvements would include advanced traffic control and mast arms where not currently located. The Forward Pinellas board is scheduled to approve funding for the highest-ranked applications at their March 8, 2023, meeting.

Map 2. Transportation Level of Service



The City of St. Petersburg is committed to maintaining a safe transportation system for all users, including pedestrians and bicyclists. A Complete Streets administrative policy was signed in November 2015 that aims to make all city streets and travel ways safe and accommodating to all modes of transportation. The Complete Streets Implementation Plan was adopted in May 2019.

**5. Appropriate and adequate land area sufficient for the use and reasonably anticipated operations and expansions;**

The subject properties allow up to four (4) units per lot (calculated at 30 units per acre) on a typical-sized 50-ft. x 127-ft. parcel. If the NTM-1 building and site design standards are not met, then the property will not be able to increase the units on the property.

**6. The amount and availability of vacant land or land suitable for redevelopment for similar uses in the City or on contiguous properties;**

The majority of the subject properties are currently developed with approximately 3.8-percent (%) of the subject properties vacant. This amendment allows for additional units to be added to the property while meeting the NTM-1 design requirements.

**7. Whether the proposed change is consistent with the established land use pattern of the areas in reasonable proximity;**

The current Future Land Use designation for the property is Planned Redevelopment-Residential (PR-R), allowing up to 30 units/acre when located outside the CHHA and when abutting a major street as depicted on the Future Major Streets Map (Map 20, Comprehensive Plan). Properties within the PR-R Future Land Use designation that are not located along Future Major Streets are allowed up to 15 units/acre. No Future Land Use amendments are required or are proposed to be changed. The requested map amendment is consistent to the PR-R designation and follows the Comprehensive Plan policies of providing a higher density of residential development along Future Major Street corridors and transit routes.

**8. Whether the existing district boundaries are logically drawn in relation to existing conditions on the property proposed for change;**

The proposed map amendment includes the properties located within the Planned Redevelopment-Residential and meet the NTM-1 locational criteria including:

- 175-feet from the centerline of a designated *Future Major Street*;
- adjacent to a public alley;
- retains direct connectivity to one or more adjoining *Future Major Streets*; and
- *a minimum of 75% of the property\** is outside of the designated Coastal High Hazard Area ("CHHA") and does not increase density in CHHA.

*\*As proposed by LDR 2023-01*

The proposed map amendment is consistent to the PR-R Future Land Use designation and NTM-1 district requirements.

**9. If the proposed amendment involves a change from residential to a nonresidential use or mixed use, whether more nonresidential land is needed in the proposed location to provide services or employment to residents of the City;**

Not applicable.

**10. Whether the subject property is within the 100-year floodplain, hurricane evacuation level zone A or coastal high hazard areas as identified in the coastal management element of the Comprehensive Plan;**

Approximately 27 properties are partially located within the 100-year floodplain and correspond to the properties that are partially located within the CHHA. However, no properties are within Hurricane Evacuation Zone A. No density increase is allowed for properties within the CHHA.

**PUBLIC OUTREACH**

Starting in August 2022, the Planning and Development Services Staff responded to individual inquiries and neighborhood association invitations and hosted six (6) workshops pertaining specifically to this ZM-15 application including:

- 11/14/2022 ZM-15/LDR 2023-01 NTM-1 Map and Text Amendments  
Workshop @ Childs Park
- 11/15/2022 ZM-15/LDR 2023-01 NTM-1 Map and Text Amendments  
Workshop @ Roberts Recreation Center
- 11/16/2022 ZM-15/LDR 2023-01 NTM-1 Map and Text Amendments  
Virtual Workshop
- 12/13/2022 CPPC ZM-15/LDR 2023-01 Map and Text Amendments Workshop
- 01/11/2023 Crescent Lake Neighborhood Association Meeting
- 01/12/2023 DRC LDR 2023-01/ZM-15 Text and Map Amendments Workshop
- 02/02/2023 Euclid/St. Paul Neighborhood Association
- 02/07/2023 CPPC ZM-15/LDR 2023-01 Map and Text Amendments Workshop
- 02/07/2023 Woodlawn Oaks Neighborhood Association
- 02/24/2023 Greater Woodlawn Neighborhood Association (scheduled)
- 02/21/2023 Lake Pasadena Estates Neighborhood Association (scheduled)
- 03/01/2023 Historic Kenwood Neighborhood Association (scheduled)

As of February 6, 2023, staff has received 196 emails and phone calls from approximately 133 people. Approximately 71 have been from individuals seeking additional information, but who did not express an opinion for or against the application. The calls and emails included 35 in opposition and 27 in support (multiple contacts from an individual were only counted once). Public feedback included a diversity of subjects, comments, and concerns of the proposed rezoning and the effects on their neighborhoods, streets, utilities and alleys. Specifically, discussion focused on the congestion of streets, potential flooding, potholes in alleys, and concerns about the maintenance of rental properties. Correspondence provided regarding the map amendments is attached.



**PUBLIC HEARING PROCESS**

The proposed zoning map amendment requires one (1) public hearing before the Community Planning and Preservation Commission and two (2) City Council public hearings.

**RECOMMENDATIONS**

Staff recommends that the Community Planning and Preservation Commission, in its capacity as the Land Development Regulation Commission, make a finding of consistency with the City’s Comprehensive Plan and recommend to City Council APPROVAL of the NTM-1 map amendments to the Official Zoning Map as illustrated.

**REPORT PREPARED BY:**

<i>Ann Vickstrom</i>	02/07/2023
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Ann Vickstrom, AICP, Planner II Urban Planning and Historic Preservation Division Planning & Development Services Department	DATE

**REPORT APPROVED BY:**

<i>Derek A. Kilborn</i>	02/07/2023
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Derek Kilborn, Manager Urban Planning and Historic Preservation Division Planning & Development Services Department	DATE

**Attachments:**

- Sectional Maps
- Public Comment Report
- LDR 2023-01: NTM-1 LDR Text Amendment

**HOUSING AFFORDABILITY IMPACT STATEMENT**

**City of St. Petersburg  
Housing Affordability Impact Statement**

Each year, the City of St. Petersburg receives approximately \$2 million in State Housing Initiative Partnership (SHIP) funds for its affordable housing programs. To receive these funds, the City is required to maintain an ongoing process for review of local policies, ordinances, resolutions, and plan provisions that *increase the cost of housing construction, or of housing redevelopment*, and to establish a tracking system to estimate the cumulative cost per housing unit from these actions for the period July 1– June 30 annually. This form should be attached to all policies, ordinances, resolutions, and plan provisions which increase housing costs, and a copy of the completed form should be provided to the City’s Housing and Community Development Department.

**I. Initiating Department:** Planning & Development Services Development

**II. Policy, Procedure, Regulation, or Comprehensive Plan Amendment Under Consideration for adoption by Ordinance or Resolution:**

See attached amendment to Chapter 16, City Code of Ordinances (City File LDR 2022-01).

**III. Impact Analysis:**

A. Will the proposed policy, procedure, regulation, or plan amendment, (being adopted by ordinance or resolution) increase the cost of housing development? (i.e. more landscaping, larger lot sizes, increase fees, require more infrastructure costs up front, etc.)

No  (No further explanation required.)

Yes  Explanation:

If Yes, the **per unit cost increase** associated with this proposed policy change is estimated to be: \$ \_\_\_\_\_.

B. Will the proposed policy, procedure, regulation, plan amendment, etc. increase the time needed for housing development approvals?

No  (No further explanation required)

Yes  Explanation:

**IV: Certification**

X: It is important that new local laws which could counteract or negate local, state and federal reforms and incentives created for the housing construction industry receive due consideration. If the adoption of the proposed regulation is imperative to protect the public health, safety and welfare, and therefore its public purpose outweighs the need to continue the community’s ability to provide affordable housing, please explain below:

The proposed regulation, policy, procedure, or comprehensive plan amendment will **not** result in an increase to the cost of housing development or redevelopment in the City of St. Petersburg and no further action is required. (Please attach this Impact Statement to City Council Material and provide a copy to Housing and Community Development department.)

/s/ Elizabeth Abernethy

Director, Planning & Development Services (signature)

02-07-2023

Date

Copies to: City Clerk; Joshua A. Johnson, Director, Housing and Community Development

**Sectional Maps**

Sectional: North

Sectional: East


Sectional: West







**Legend**

 NTM-1 Zoning (PR-R)



Date: 01-20-23

Planning and Development Services  
City of St. Petersburg

**NTM-1 (Neighborhood Traditional Mixed Residential) Zoning**

**Public Comments Report**

Public Comment Registry

Emails Support

Emails Opposed

## City File: ZM-15

### Public Comment Registry

Through 02/06/23

CPPC Public Hearing February 14, 2023

Date	#	Type	Name	Status
11/10/2022	1	Tel	Tony Rodriquez	Info
11/10/2022	2	Eml	Zach Zehnder	Info
11/10/2022	3	Tel	Kara King	Info
11/10/2022	4	Tel	Bibi Ganie	Info
11/10/2022	5	Eml	Zack Zehnder	Info
11/10/2022	6	Eml	Zack Zehnder	Info
11/10/2022	7	Eml	Mark Holguin	Info
11/10/2022	8	Eml	Michael Szabo	Info
11/10/2022	9	Eml	Michael Szabo	Info
10/10/2022	10	Eml	Dan Kowalski	Info
11/11/2022	11	Eml	Nick Price	Info
11/11/2022	12	Eml	Mark Holguin	Info
11/15/2022	13	Eml	Mindy Durkin	Info
11/15/2022	14	Eml	Kowalski	Info
11/15/2022	15	Eml	Willingham	Info
11/15/2022	16	Eml	Kowalski	Info
11/15/2022	17	Eml	Kowalski	Info
11/18/2022	18	Eml	PCPAO Wright	Info
11/13/2022	19	Eml	Gina Marie Foti	Supports
11/14/2022	20	Tel	Henry Berdat	Info, Support
11/15/2022	21	Eml	Mindy Durkin	Info
11/15/2022	22	Eml	Lisa Presnail	Info - Opposed
11/15/2022	23	Tel	Ethel Bentley	Info
11/16/2022	24	Tel	Sandie Foster	Supports
11/16/2022	25	Tel	Anne Duran	Info
11/16/2022	26	Eml	Kowalski	Info
11/16/2022	27	Eml	Elizabeth Vogt	Info
11/16/2022	28	Eml	Gail Mathews	Info - Supports
11/17/2022	29	Tel	Henry Berdat	Info - Supports
1/5/2023	30	Eml	Gina Marie Foti	info
1/5/2023	31	Eml	Gina Marie Foti	info
1/3/2023	32	Eml	Gina Marie Foti	info
1/3/2023	33	Eml	Gina Marie Foti	info
1/6/2023	34	Eml	Gina Marie Foti	info
1/9/2023	35	Eml	Gina Marie Foti	info
1/9/2023	36	Eml	Gina Marie Foti	info
1/10/2023	37	Eml	Gina Marie Foti	info
1/10/2023	38	Eml	Gina Marie Foti	info
1/10/2023	39	Eml	Gina Marie Foti	info
1/10/2023	40	Tel	Green	Info
1/10/2023	41	Tel	Green	Info
1/10/2023	42	Tel	Green	Info

**City File: ZM-15****Public Comment Registry**

1/10/2023	43	Tel	Liz	Info
1/10/2023	44	Tel	Ed McGrath	Info
1/10/2023	45	Tel	Beth Murphy	Opposed
1/10/2023	46	Tel	Paul Hudson	Info
1/11/2023	47	Tel	Rose Gullet	Info
1/11/2023	48	Tel	Angela Hamilton	Info
1/11/2023	49	Tel	Tim	Supports
1/11/2023	50	Tel	Rose Brown	Info
1/11/2023	51	Eml	Palmetto Park Neighborhood Assoc	Supports
1/11/2023	52	eml	Herzfeld	Info
1/11/2023	53	Eml	Gina Marie Foti	Info Support
1/14/2023	54	Eml	John Potts	Opposed
1/15/2023	55	Eml	Payne	Supports
1/15/2023	56	Eml	Jennifer Teolis	Opposed
1/15/2023	57	Eml	Madeline Gulliver	Info
1/15/2023	58	Eml	Thomas Ley	Info Opposed
1/15/2023	59	Tel	Matthew May	Info
1/15/2023	60	Tel	Mellissa	Info
1/15/2023	61	Tel	George Busack	Info
1/15/2023	62	Tel	Justine Rhodes	Info
1/15/2023	63	Tel	Mr. Waznegger	Info
1/15/2023	64	Tel	Samuel Davis	Info, Opposed
1/15/2023	65	Tel	Eva Kowalewski	Opposed
1/15/2023	66	Tel	Beth Murphy	Opposed
1/15/2023	67	Tel	Carmen Prime	Opposed
1/15/2023	68	Tel	Saffrita	Info
1/15/2023	69	Tel	Rose Gullet	Info
1/15/2023	70	Tel	Laryssa Woodward	Supports
1/15/2023	71	Tel	Angela Hamilton	Info
1/15/2023	72	Tel	Jennifer	Info, Support
1/15/2023	73	Tel	Debra Scanlon	Info
1/15/2023	74	Tel	Samuel Davis	Info
1/15/2023	75	Tel	Venji	Info, Support
1/16/2023	76	Eml	Michele Angermeier	Opposed
1/16/2023	77	Eml	Greg Tappan	Opposed
1/16/2023	78	Eml	Michael Winterbottom	Opposed
1/16/2023	79	Eml	Jennifer Ryan-Molesky	Info
1/15/2023	80	Eml	Leroy Green	Info
1/16/2023	81	Eml	Heather Grzelka	Opposed
1/17/2023	82	Eml	Madeline Gulliver	Info
1/17/2023	83	Eml	Ken Rikard	Info
1/17/2023	84	Eml	David Delrahim	Info
1/17/2023	85	Eml	Michele Angermeier	Opposed
1/17/2023	86	Eml	Historic Kenwood NA	Info
1/17/2023	87	Eml	Jalessa Blackshear	Info
1/17/2023	88	Eml	Ryan Silveria	Supports
1/17/2023	89	Eml	Dan Kowalski	Supports



**City File: ZM-15****Public Comment Registry**

1/17/2023	90	Eml	Bruno Fernandez	Info
1/17/2023	91	Eml	Kelli Quincel	Info
1/17/2023	92	Eml	Dustin Baldwin	Info, Support
1/17/2023	93	Eml	Deborah Martohue	Opposed
1/18/2023	94	Tel	Katherine Nichol	Info
1/18/2023	95	Tel	Scot Andariese	Info, Support
1/18/2023	96	Tel	Saffita	Info, Support
1/18/2023	97	Tel	Judy Windish	Opposed
1/18/2023	98	Tel	Brad	Info, Support
1/18/2023	99	Tel	Charles Samaha	Info
1/18/2023	100	Tel	Melissa	Info
1/18/2023	101	Tel	Mary Ann	Info, Support
1/18/2023	102	Tel	Pete Wilkins	Opposed
1/19/2023	103	Tel	Dan Khoury	Info
1/19/2023	104	Tel	Andrew Davis	Info
1/19/2023	105	Tel	Pat Davis	Opposed
1/19/2023	106	Tel	Sharon Ingram	info, Support
1/19/2023	107	Tel	Bernice William	Info
1/19/2023	108	Tel	Beth Murphy	Info, Opposed
1/19/2023	109	Tel	Jen	Info
1/19/2023	110	Tel	Kathleen Stroud	Info, Opposed
1/19/2023	111	Tel	Erik	Supports
1/19/2023	112	Tel	Natham Gray	Supports
1/19/2023	113	Tel	Leontine DeBarbiery	Opposed
1/19/2023	114	Eml	David Delrahim	Info
1/19/2023	115	Eml	Vicki Morgan	Info
1/19/2023	116	Eml	Jalessa Blackshear	Info
1/19/2023	117	Eml	Matthew May	Info
1/19/2023	118	Eml	Lisa Lippincott	Info
1/19/2023	119	Eml	Mark & Cynthia Stephens	Info
1/19/2023	120	Eml	Ky Fer	Info
1/19/2023	121	Eml	John Potts	Info, Opposed
1/19/2023	122	Eml	Bob Singer	Info
1/19/2023	123	Eml	Dustin Baldwin	Info, Support
1/19/2023	124	Eml	Deborah Martohue	Opposed
1/19/2023	125	Eml	Woodlawn Oaks NA/Andy Davis	Info
1/19/2023	126	Eml	Toby Duffield	Opposed
1/19/2023	127	Eml	Thomas Ley	Opposed
1/19/2023	128	Eml	Bob Singer	Info
1/19/2023	129	Eml	Kate Zamboni	Opposed
1/22/2023	130	Tel	Jamie Dickerson	Info
1/22/2023	131	Tel	Jeanette Keys	Supports
1/22/2023	132	Eml	Woodlawn Oaks NA	Info
1/23/2023	133	Eml	Norm Zamboni	Opposed
1/23/2023	134	Eml	Kate Zamboni	Opposed
1/23/2023	135	Eml	Jamie Dickerson	Opposed
1/23/2023	136	Eml	Goran Ivanov	Opposed
1/23/2023	137	Eml	Joseph Mingione	Supports



**City File: ZM-15****Public Comment Registry**

1/23/2023	138	Eml	Barbara Nicolaisen	Opposed
1/23/2023	139	Eml	Michael Williams	Info
1/23/2023	140	Eml	Mark & Cynthia Stephens	Info
1/23/2023	141	Eml	Gayle Mathews	Supports
1/23/2023	142	Eml	Elizabeth Vogt	Info
1/24/2023	143	Tel	Janet Young	Info
1/24/2023	144	Tel	Leroy Green	Info
1/24/2023	145	Tel	Sarah Wine	Info, Support
1/24/2023	146	Tel	Michael Williams	Info
1/24/2023	147	Tel	Henry Verdat	Info, Support
1/24/2023	148	Eml	Woodlawn Oaks	Info
1/24/2023	149	Eml	Carol Gruszka	Opposed
1/24/2023	150	Eml	Lane Driscoll	Opposed
1/24/2023	151	Eml	Karen Lorenz	Opposed
1/25/2023	152	Tel	Mark McGrath	Info
1/25/2023	153	Tel	Jennifer Larmen	Opposed
1/29/2023	154	Eml	Robin Reed	Info
1/30/2023	155	Tel	James Golin	Supports
1/30/2023	156	Eml	Niel Allen	Info
1/30/2023	157	Tel	Franklin Motley	Supports
1/30/2023	158	Eml	Robin Reed	Info
1/30/2023	159	Eml	Robin Reed	Info
1/30/2023	160	Eml	ESPNA	Info
1/30/2023	161	Eml	Niel Allen	Info
1/30/2023	162	Eml	Bill McCain & Jeff Kenner	Info
1/30/2023	163	Eml	GWNA	Info
1/30/2023	164	Eml	Norma Bouillion	Opposed
1/30/2023	165	Eml	Mike Battigelli	Opposed
1/30/2023	166	Eml	Corey Vongsalay	Opposed
1/30/2023	167	Eml	Susan Fulmer	Opposed
1/30/2023	168	Eml	John Deas	Info
1/30/2023	169	Eml	Bob Watson	Info
1/30/2023	170	Eml	Grant Crockett	Incorrect Application
1/30/2023	171	Eml	Cathy Wilson	Opposed
1/30/2023	172	Tel	Scott Seaman	Info
1/30/2023	173	Eml	Susan Fulmer	Opposed 2nd
2/1/2023	174	Eml	Gina Marie Foti	Supports
2/1/2023	175	Eml	Ryan Smith	Supports
2/1/2023	176	Eml	Woodlawn Oaks NA	Info
2/1/2023	177	Eml	Robbie Griffie	Info
2/1/2023	178	Eml	Dan Kowalski	Support Info
2/1/2023	179	Tel	Raphael Perrier	Opposed
2/1/2023	180	Tel	Patrick Ruffin	Support
2/1/2023	181	Tel	Lisa Till	Info
2/1/2023	182	Tel	Carol Terroni	Opposed
2/1/2023	183	Tel	Andy Schwartz	Support
2/1/2023	184	Tel	Jillian Redford	Opposed
2/1/2023	185	Tel	Dan Moranda	Info

**City File: ZM-15**

**Public Comment Registry**

2/1/2023	186	Tel	Nicole Harder	Info
2/1/2023	188	Tel	Dale Marks	Info
2/6/2023	189	Tel	Lisa Mclusky	Info
2/6/2023	190	Tel	Linda Aure	Info
2/6/2023	191	Tel	Sharon Newton	Info
2/6/2023	192	Tel	Allison Swift	Info

December 9, 2022

Gina Marie Foti  
2643 5th Ave South, St. Petersburg, Florida, 33712  
[Imaginationten@gmail.com](mailto:Imaginationten@gmail.com) | (727) 902-1252

Dear Development Review Commission,

**NTM-1 Property:** 2643 5th Ave South, St. Petersburg, Florida, 33712

**Re:** Request to Maintain Front Parking and Access in Former Church / Home in Qualified NTM-1 Rezoning, as Part of Ongoing Efforts to Mitigate St. Petersburg's Housing Crisis

I am pleased to say that my home qualifies and fits all the requirements for the NTM-1\* rezoning.

Although I fully support and commend the City for these efforts, my home presents a unique scenario that requires thoughtful consideration. My property was previously a small neighborhood church with cultural significance, and was built and designed substantially further back on the lot than the surrounding homes to allow front access parking (see Exhibit A).

As it stands, the NTM-1 proposal states that all required parking spaces must be accessed exclusively from the alley. Therefore, if I take advantage of the proposed NTM-1 zoning, it may require the removal of my home's existing front parking and access.

### **Request**

Given the unique nature of my home, formerly a church, I am asking for the NTM-1 zoning to be amended to allow the property to maintain its front parking access.

### **Benefits of Approving Zoning Request**

If my request is approved, it will provide the following benefits to the community:

- Encourage the modification of an existing property, while complementing the structure, its unique nature and cultural neighborhood past.
- Allow a St. Petersburg resident to participate in the NTM-1 rezoning program.
- Promote walkability, "at the core of the success of increasing walkability is density, it is the key to making these communities walkable and vibrant." Urban Land Institute\*\*
- Prevent burdensome, costly and unknown variances.
- Benefit the community with additional dwelling units, and therefore support NTM-1's primary objective.

On the other hand, if I am required to remove the front parking, or have to go through unknown variances, it would put me at a great disadvantage, and would encourage the removal of the existing structure, creating an unnecessary, expensive, and time-consuming burden, significantly delaying the potential for additional units that can become available to the community.

### **Conclusion**

While I appreciate the parking parameters in the NMT-1 rezoning for most case scenarios, I urge you to allow my request to amend the NTM-1 rezoning and allow my property to maintain its front parking access. If we work together, we can develop and sharpen many wonderful tools to solve the housing crisis in St. Petersburg.

The NTM-1 proposed change aims to increase housing supply, and to "enable various dwelling units in

response to market demands, encouraging the use and preservation of current structures.” I purchased a former church and not a regular single-family home as housing shortages have us all exploring creative ways to address the situation.

### **Community Support**

Attached is a letter from the President of the Palmetto Park Neighborhood Association, supporting retaining the parking and front access of the property due to its unique nature (See Exhibit B).

Also attached, is a letter from Bishop Zema Florence. prior owner, and operator of the community church, supporting my request. As you will read in his letter, it is of importance to him to have part of this structure be included in the new proposed NTM-1 zoning, and feels strongly that allowing the front parking and entrance to remain on the site will facilitate additional units to the community (See Exhibit C).

I appreciate your consideration.

Sincerely,

*Gina Marie Foti*

### **Gina Marie Foti**

Homeowner  
2643 5th Ave South  
St. Petersburg, Florida

\*Under this proposed change, single-family houses may expand to include accessory dwelling units (e.g. garage apartments) or be redeveloped up to a maximum four (4) residential units. These units may be developed as rental apartments, townhouses, or condominiums.) Source: City of St. Petersburg website ([https://www.stpete.org/residents/current\\_projects/planning\\_projects/housing\\_initiatives.php](https://www.stpete.org/residents/current_projects/planning_projects/housing_initiatives.php))

\*\* The Benefits of Growth - Urban Land Institute. 1 Mar. 2019, [https://uli.org/wp-content/uploads/ULI-Documents/BenefitsofGrowth.ashx\\_.pdf](https://uli.org/wp-content/uploads/ULI-Documents/BenefitsofGrowth.ashx_.pdf).

### **Exhibit A:**



**See Support Letters in the Following Pages**

## Exhibit B

Kate M Thorpe-Eddleman  
President, Palmetto Park Neighborhood Association  
thorpekatem@gmail.com  
(786) 553-9881

December 8, 2022

Dear City of St. Petersburg,

- Request to Maintain Front Parking and Access in Qualified NTM-1 Rezoning Home / Former Church: 2643 5th Ave South, St. Petersburg, Florida, Palmetto Park Neighborhood Association
- Show support NTM-1 Rezoning

As a devoted resident and President of the Palmetto Park Neighborhood Association, I am constantly seeking ways to improve the quality of life in our neighborhood and surrounding areas. I praise the city's plans to expand NTM-1 zoning to alleviate the housing crisis facing our community.

I am writing this letter of support for a unique scenario concerning a Palmetto Park Neighborhood Association home/former church. The NTM-1 rezoning proposal currently states that all parking must be in the back alley. However, this property, originally a small church and now a residential home, was built significantly further back on the lot to also accommodate front parking. Therefore, while the property fully qualifies for the NTM-1 rezoning, it may require the removal of existing front parking and access.

**Given the unique nature of the property, I am asking for the NTM-1 zoning proposal to allow the current owner to maintain the front parking and access, retaining her property's qualification of the proposed rezoning.** Allowing this will remove a significant obstacle for the owner and facilitate the creation of additional residential units on the property.

I am confident that you will grant this request, recognizing that it will encourage the modification of a unique, and important structure in the Palmetto Park Neighborhood, while allowing the community to benefit from additional dwelling units through NTM-1's rezoning.

I look forward to your decision.

Sincerely,

*Kate M Thorpe Eddleman*

December 12, 2022

Kate M Thorpe-Eddleman  
President, Palmetto Park Neighborhood Association

Bishop Zema J. Florence  
The Holy Temple of Church  
(727) 452-3191 | florencezema@gmail.com

December 9, 2022

**Re:** Request to Maintain Front Parking and Access in Former Church / Home for Qualified NTM-1 Property

**Subject Property:** Qualified NTM-1 Rezoning Property, 2643 5<sup>th</sup> Ave South, St. Petersburg, Florida, 33712

To Whom It May Concern:

This letter is regarding my former property and church (now a residence), 2643 5<sup>th</sup> Ave South, St Pete, Florida 33712, which I am pleased to say qualifies for the NTM-1 rezoning proposal.

As the previous owner and operator of the church, formerly known as the Holy Temple Cathedral Church of God in Christ, it is important to me that the property continues to benefit the community which it served for years, but now as additional housing.

Since the property was built as a small church, it was constructed further back on the lot to accommodate for front access and parking. While the property fully qualifies for the NTM-1 rezoning, it may require the removal of the existing front parking and access. **Given the unique nature of the property, I am asking for the NTM-1 zoning proposal to allow the current owner to maintain the front parking access.**

Allowing the front parking and entrance will make it easier for the home to be modified to accommodate additional units. My concern is that if the front parking is required to be removed, it will create costly obstacles to the new owner and may incentivize not using any of the current structure.

It was a difficult decision to sell the church, but we secured a larger location to accommodate our growing congregation. I sold Ms. Foti the property, because she cares about the community, knowing it would be in good hands.

Prior to my time as Bishop, the church had played a key role in St. Petersburg, as part of the Living God Worldwide Revival Center. Its founding father, Pastor Samuel Butler, an influential gospel musician and his sons, recorded with the Five Blind Boys of Alabama, one of the groups which helped end segregation in the United States.

During my time at the church, we served hundreds of members with holiday celebrations and gatherings, food drives, and clothes giveaways, welcoming key members of our community to participate in our services, including the late Reverend Watson Haynes, III as one of our keystone Black History Month speakers.

Modifying the property to provide housing will be an incredible part of its journey, allowing it to continue to serve its community. I urge you to allow the home to retain its front parking and entrance, so that the current owner does not have unnecessary hardships in order to benefit from NTM-1.

Sincerely,



Bishop Zema J Florence

December 12, 2022

## Ann O. Vickstrom

---

**From:** Kate M Thorpe-Eddleman <thorpekatem@gmail.com>  
**Sent:** Wednesday, January 11, 2023 12:04 PM  
**To:** mkiernan@traublieberman.com; Tim Clemmons; mwalker@georgefyoung.com; mrutland@ccim.net; ksingleton@fathomrealty.com; jopseh@griner-engineering.net; John C. Muhammad; Copley C. Gerdes; Brandi J. Gabbard; Ed Montanari; Lisset G. Hanewicz; Deborah D. Figgs-Sanders; Gina L. Driscoll; Richmond J. Floyd; gina@ginaswarehouse.com; James A. Corbett; Kenneth T. Welch; Mayor; Council; Elizabeth Abernethy; Ann O. Vickstrom; Derek Kilborn; Corey D. Malyszka; Brian Caper; Robert M Gerdes; Tom Greene; Amy E. Foster; devrev; Cindy Sheppard  
**Subject:** Important: Palmetto Park Neighborhood/Warehouse Arts District NTM-1  
**Attachments:** image0.jpeg; image1.jpeg; GW\_RezoningChurchLetter.pdf; GW\_RezoningChurchLetterKpdf.pdf

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Morning,

My name is Kate M Thorpe-Eddleman, President of the Palmetto Park Neighborhood Association. Please find attached my letter of support, along with 20 signatures from Palmetto Park residents in support for the City of St. Petersburg to pass a text amendment for properties that are within the proposed NTM-1 zoning but were formerly churches/ institutional homes, allowing them to retain existing front parking and access. I am all for development and growth of St Pete, but it is important to give a thoughtful approach and solution to existing properties and residents as we do so.

One of these significant properties is located at 2643 5th Ave S, St Pete 33712, which qualifies for the proposed NTM-1 zoning. This property was originally developed as a small church with cultural significance and was this developed all the way to the back on the lot then the surrounding homes, to allow front access parking.

A text amendment to the NTM-1 rezoning proposal would allow the average homeowner who purchased a former church or other former institutional properties (like schools) to have the opportunity to be a part of St. Pete's growth, and would encourage the retention of these already existing structures. Please see attached

It's an important time in our area. This former church (now home) is on 5th Ave S in Palmetto Park, in one of the oldest neighborhoods in St Pete, only a few blocks away from Gas Plant/Trop/22nd S and also in the Warehouse Arts District.

Tomorrow at 11, the DRC is meeting/workshop, and the City is presenting NTM-1 zoning and text amendments to the DRC and requesting this amendment be included. It is a time when we need to come together to solve this with an amendment before it gets passed.



Thank you,

Kate M Thorpe-Eddleman

## Ann O. Vickstrom

---

**From:** Corey D. Malyszka  
**Sent:** Thursday, January 12, 2023 11:14 AM  
**To:** Derek Kilborn; Elizabeth Abernethy  
**Cc:** Ann O. Vickstrom  
**Subject:** FW: Proposed Solution: Zoom DRC Workshop NTM-1 Amendment Request  
**Attachments:** 1.10.23.pdf; GW\_RezoningChurchLetter.pdf; Thorpe-Eddleman\_SupportLetter.pdf; BishopFlorence\_SupportLetter (1).pdf

Below email was just received.

---

**From:** Gina Marie Foti <gina@ginaswarehouse.com>  
**Sent:** Thursday, January 12, 2023 11:07 AM  
**To:** Corey D. Malyszka <Corey.Malyszka@stpete.org>  
**Subject:** Proposed Solution: Zoom DRC Workshop NTM-1 Amendment Request

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Corey,

Solving the housing crisis takes effort, collaboration and thoughtful creative approaches and can be simple but not always easy, we would love your help with taking a deeper look into solutions for the proposed issue (and possible solution) below.

Attached supporting including community supportive signatures, and support letters from the association, Bishop, along with pictures, and a detailed outline. (attached)

### As a proposed solution:

Please consider introducing as a possible amended text option to allow for properties developed as small churches in the NTM-1 zoning with existing front access parking to retain their parking.

*The amended text is not for all front access properties and can have clear parameters, such as*

Developed and used as a church

Front entry was prior to 2023

Single lot

zoned residential 4 units or less and not commercial.

No side driveway

(along with some thoughtful boundaries and consideration.  
Including requiring extra landscaping)

### Benefits:

The amendment to the NTM-1 zoning allow the property to maintain its existing front parking and access, which would provide several benefits to the St. Petersburg community:

Encourage the modification of an existing property, while complementing the structure, its unique nature and cultural neighborhood past, rather than incentivizing property owners to demolish them and build new structures to fit new zoning criteria.

Create less obstacles for additional affordable house opportunities.

Allow a St. Petersburg resident to participate in the NTM-1 rezoning program.

Promote walkability by increasing density, "at the core of the success of increasing walkability is density, it is the key to making these communities walkable and vibrant." Urban Land Institute

Prevent burdensome, costly and unknown variances.

And Benefit the community with additional dwelling units, and therefore support NTM-1's primary objective.

We believe that there needs to be more thoughtful consideration for former institutional properties, such as churches or schools, that have been converted to residences - there has only been data collected for current churches not former churches now residences. This amendment would solve these issues and help retain these properties by making it easier to utilize them for what the NTM-1 was promoted to do.

Thank you for your consideration,  
Gina Marie Foti

## Ann O. Vickstrom

---

**From:** Will Payne <will.payne12@gmail.com>  
**Sent:** Sunday, January 15, 2023 8:56 AM  
**To:** Ann O. Vickstrom; Lisset G. Hanewicz; Jayne E. Ohlman  
**Subject:** Support for Proposed Zoning Map Changes

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Morning,

I wanted to email to register my wholehearted support for the upcoming proposed zoning map changes amending Neighborhood Traditional to Neighborhood Traditional Mixed Residential.

This change to allow a greater diversity of housing types, particularly near major corridors, will enable more units of housing in a way that will address growing affordability challenges here in St. Petersburg. I don't want my neighborhood to become less socioeconomically diverse because a large segment of buyers are priced out of buying or renting homes here (or anywhere). I'm hopeful it will provide benefits to businesses along our corridors, who will have more customers in the immediate vicinity and lead to more thriving, walkable neighborhood shops and restaurants I can enjoy without driving.

I currently own a single-family home, and my immediate next door neighbors are an older, 50's fourplex. My neighbors are absolutely wonderful, long-time residents and thoughtful, considerate neighbors. My experience has shown that the combination of housing types has no bearing on the quality of our neighborhoods; in fact, I think it improves them.

As St. Pete, the State of Florida, and our world changes, our neighborhoods must change too. This change is an important one to allow St. Pete to keep pace with growth in a way that will only enhance our neighborhoods for the future.

**Thank you** for what I'm sure has been an immense amount of behind-the-scenes work putting these changes together and count me as a neighbor who is excited to see them enacted.

Will Payne  
777 29th Ave N

## Ann O. Vickstrom

---

**From:** Silveira, Ryan <Ryan.Silveira@gea.com>  
**Sent:** Tuesday, January 17, 2023 11:27 PM  
**To:** Ann O. Vickstrom  
**Subject:** Re: Question in Regards to Property

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you Ann! Very happy the city seems to be moving forward on this big issue of housing. I also own property at 948 26th st n and 2635 14th ave n. Will these properties be changing zoning as well? I received the card in the mail about this but not sure which properties apply. Thank you!

Get [Outlook for iOS](#)

---

**From:** Ann O. Vickstrom <Ann.Vickstrom@stpete.org>  
**Sent:** Monday, November 28, 2022 9:48:16 AM  
**To:** Silveira, Ryan <Ryan.Silveira@gea.com>  
**Subject:** RE: Question in Regards to Property

**THIS MESSAGE IS FROM AN EXTERNAL SENDER**

**Please be cautious, particularly with links and attachments**

Ryan,

Good morning! Thank you for contacting the City Planning and Development Services Department concerning your property.

As you indicated, your property at 2144 5<sup>th</sup> Avenue N is zoned Corridor Residential Traditional -1 (CRT-1). The CRT-1 zoning district allows up to 24 units/acre. Based on the Pinellas County Property Appraiser website, your property is approximately 0.123 acre, allowing up to 3 units. There is a Work Force Housing density bonus of 8 units/acre which would allow for 1 additional bonus workforce housing following the specific criteria. Therefore, at this time, you are allowed 3 units plus one Workforce Housing bonus unit for a total of 4 units. The Work Force Housing unit requires a contract with the city. I have attached the City Code Article V which provides the criteria for Work Force Housing. Please contact Stephanie Lampe at [Stephanie.Lampe@stpete.org](mailto:Stephanie.Lampe@stpete.org) for more information concerning Work Force Housing. In addition, we are currently proposing to amend the CRT-1 district to allow Missing Middle development up to 30 units/acre allowing for duplexes, triplexes and fourplexes. This proposal is scheduled for City Council review in March 2023.

Please let me know if you have additional questions.

Respectfully,

Ann Vickstrom, AICP, RLA#0001122

Planner II, Urban Planning and Historic Preservation

City of St. Petersburg

(727)892-5807

[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)

Please note all emails are subject to public records law.

---

**From:** Silveira, Ryan <Ryan.Silveira@gea.com>  
**Sent:** Tuesday, November 22, 2022 12:50 PM  
**To:** Ann O. Vickstrom <Ann.Vickstrom@stpete.org>  
**Subject:** Question in Regards to Property

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Ann,  
I own a two unit property at 2144 5<sup>th</sup> Ave N. I know the city is trying to incentivize the building of more units and allowing more flexibility. A recent project two doors down subdivided a lot and made two townhomes on a single lot by Canopy Builders. My question is now that my property is rezoned to CRT-1...am I able to build 4 units? Or would this only be if workforce housing was included? Also, how is the city incentivizing workforce housing? Is it by allowing the extra unit or are they partnering with homeowners to make this happen?

Thanks  
Best regards,

**Ryan Silveira**

**Regional Sales Engineer - Southern Region**

Environmental Market  
Region NAM | North America  
Email [Ryan.Silveira@gea.com](mailto:Ryan.Silveira@gea.com)  
Mobile +1 (561) 908- 3091  
Landline+1 (863) 603-8920  
Web [www.gea.com](http://www.gea.com)



**GEA Mechanical Equipment US, Inc.**

100 Fairway Court Northvale, NJ 07647, United States  
Management Board: Michael Vick, Jeffrey Scholl  
Registered Office: 1209, Orange Street, Wilmington, DE 19801, United States  
Register Details: The Corporation Trust Company, 4905589  
[Follow us on](#)



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Confidentiality note Data Protection

## Ann O. Vickstrom

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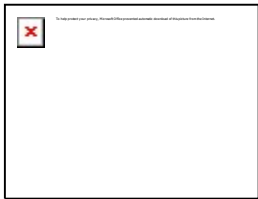
**From:** Dustin Baldwin <Dustin@velocityvg.com>  
**Sent:** Wednesday, January 18, 2023 4:51 PM  
**To:** Ann O. Vickstrom  
**Subject:** NTM Zoning Question

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Mrs. Vickstrom,

I have a lot that I am in planing and design on for 3 units (Triplex) in the CRT-1 zoning. I just noticed (after getting the postcard) that it is in the proposed NTM zoning. Does this mean I can develop 4 units here now? The parcel is under Blue Door Building CO LLC on 4th St S.

Thank you for the assistance!



**Dustin Baldwin**  
Founder/CEO at VVG

**M** [727.421.8228](tel:727.421.8228)

**E** [dustin@velocityvg.com](mailto:dustin@velocityvg.com)





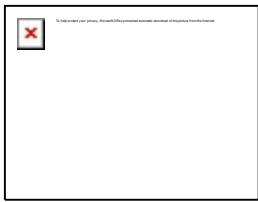
## Ann O. Vickstrom

---

**From:** Dustin Baldwin <Dustin@velocityvg.com>  
**Sent:** Friday, January 20, 2023 11:15 AM  
**To:** Ann O. Vickstrom  
**Subject:** Re: NTM Zoning Question

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you for the information, very helpful! Have a great weekend Ann.



**Dustin Baldwin**  
Founder/CEO at VVG

**M** [727.421.8228](tel:727.421.8228)

**E** [dustin@velocityvg.com](mailto:dustin@velocityvg.com)



On Jan 20, 2023, at 10:56, Ann O. Vickstrom <Ann.Vickstrom@stpete.org> wrote:

Dustin,

Thank you. The property is currently zoned CRT-1 and while we are not proposing to rezone that property to NTM-1, we are proposing a text amendment to allow any CRT-1 property an option of using a "Missing Middle Housing " bonus of up to 6 units/acre if following all NTM-1 locational, and design parameters. That would allow the same 30 units/acre as the NTM-1 zoning designation. No other housing bonuses are allowed. This is proposed for final public hearing on March 23, so we will have to wait on the outcome of City Council vote. This is an optional allowance, otherwise, a developer can follow the CRT-1 requirements. However, I have included the NTM-1 district requirements for your information.

Respectfully,

Ann Vickstrom, AICP, RLA#0001122  
Planner II, Urban Planning and Historic Preservation  
City of St. Petersburg  
(727)892-5807

[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)

Please note all emails are subject to public records law.

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**From:** Dustin Baldwin <Dustin@velocityvg.com>  
**Sent:** Friday, January 20, 2023 9:55 AM  
**To:** Ann O. Vickstrom <Ann.Vickstrom@stpete.org>  
**Subject:** Re: NTM Zoning Question

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good morning Ann, it doesn't have an address yet, 2525 4th St S will get you there via Google Maps, or if you look up Blue Door Building Co via property appraiser it will bring up the 4th St S parcel number.  
<image001.jpg>



**Dustin Baldwin**  
Founder/CEO at VVG

---

**M** [727.421.8228](tel:727.421.8228)

**E** [dustin@velocityvg.com](mailto:dustin@velocityvg.com)

On Jan 20, 2023, at 08:22, Ann O. Vickstrom <[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)> wrote:

Dustin,  
Can you give me an address for this property?  
Thank you,  
Ann Vickstrom, AICP, RLA#0001122  
Planner II, Urban Planning and Historic Preservation  
City of St. Petersburg  
(727)892-5807

[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)

Please note all emails are subject to public records law.

---

**From:** Dustin Baldwin <[Dustin@velocityvg.com](mailto:Dustin@velocityvg.com)>  
**Sent:** Wednesday, January 18, 2023 4:51 PM  
**To:** Ann O. Vickstrom <[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)>  
**Subject:** NTM Zoning Question

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Mrs. Vickstrom,  
I have a lot that I am in planing and design on for 3 units (Triplex) in the CRT-1 zoning. I just noticed (after getting the postcard) that it is in the proposed NTM zoning. Does this mean I can develop 4 units here now? The parcel is under Blue Door Building CO LLC on 4th St S.

Thank you for the assistance!



**Dustin Baldwin**  
Founder/CEO at VVG

---

**M** [727.421.8228](tel:727.421.8228)

**E** [dustin@velocityvg.com](mailto:dustin@velocityvg.com)

<SECTION\_16.20.015.\_\_\_\_NEIGHBORHOOD\_TRADITIONAL\_MIXED\_RESIDENTIAL\_DISTRICTS\_\_\_\_NTM\_\_\_\_  
(3).docx>

## Ann O. Vickstrom

---

**From:** Nathan Gray <nathan@pivotrei.com>  
**Sent:** Friday, January 20, 2023 4:09 PM  
**To:** Ann O. Vickstrom  
**Cc:** Amie Gray; Kristy McUmber  
**Subject:** ZM-15: NTM-1 Rezoning Application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Ann,

My name is Nathan Gray and I am writing on behalf of Amie Gray, owner/manager of Pivot REI. (cc'd here).

We currently own parcel ID: (273116754020001480) The parcel is a double lot, with frontage on 16th Ave S & 35th St S in St. Petersburg.

We received the notification for the rezoning application that is going across many parts of St. Petersburg. The purpose of this email is to request our parcel be added to the rezoning application. Please let me know if there is another process on how to formally apply for this request if there is another route for the request.

The reasoning for this request is based on the following detail:

- 1). Parcel ID: (273116754020001740) is a grandfathered corner store which is a recorded neighborhood nuisance. It is unusual for the area, and has affected our property adversely due to its proximity to commercial use. We feel as though our property may meet certain guidelines in the rezoning application due to it being adjacent to commercial use. The adjacent parcel also represents densities that are beyond the present-day planning maximums.
- 2). There are parcels that have been identified for rezoning to NTM-1 on 37th St S, such as parcel ID: (273116792000000010) and surrounding parcels. These parcels are adjacent to a 60' ROW with alley access. Our parcel also is adjacent to a 60' ROW with alley access. Our parcel also is a corner lot adjacent to another 60' ROW. We feel as though our access is sufficient for the proposed density and we have greater access than some parcels currently proposed.
- 3). We are active real estate developers and would be further incentivized to invest in the St. Petersburg area. We have interests in developing this parcel, and with this rezoning attached to it, it would allow our small business to further develop.

I hope to hear back and appreciate your time for attending to our parcel.

Thank you,

Nathan Gray

904-610-6340

## Ann O. Vickstrom

---

**From:** Nathan Gray <nathan@pivotrei.com>  
**Sent:** Friday, January 20, 2023 4:09 PM  
**To:** Ann O. Vickstrom  
**Cc:** Amie Gray; Kristy McUmbler  
**Subject:** ZM-15: NTM-1 Rezoning Application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Ann,

My name is Nathan Gray and I am writing on behalf of Amie Gray, owner/manager of Pivot REI. (cc'd here).

We currently own parcel ID: (273116754020001480) The parcel is a double lot, with frontage on 16th Ave S & 35th St S in St. Petersburg.

We received the notification for the rezoning application that is going across many parts of St. Petersburg. The purpose of this email is to request our parcel be added to the rezoning application. Please let me know if there is another process on how to formally apply for this request if there is another route for the request.

The reasoning for this request is based on the following detail:

- 1). Parcel ID: (273116754020001740) is a grandfathered corner store which is a recorded neighborhood nuisance. It is unusual for the area, and has affected our property adversely due to its proximity to commercial use. We feel as though our property may meet certain guidelines in the rezoning application due to it being adjacent to commercial use. The adjacent parcel also represents densities that are beyond the present-day planning maximums.
- 2). There are parcels that have been identified for rezoning to NTM-1 on 37th St S, such as parcel ID: (273116792000000010) and surrounding parcels. These parcels are adjacent to a 60' ROW with alley access. Our parcel also is adjacent to a 60' ROW with alley access. Our parcel also is a corner lot adjacent to another 60' ROW. We feel as though our access is sufficient for the proposed density and we have greater access than some parcels currently proposed.
- 3). We are active real estate developers and would be further incentivized to invest in the St. Petersburg area. We have interests in developing this parcel, and with this rezoning attached to it, it would allow our small business to further develop.

I hope to hear back and appreciate your time for attending to our parcel.

Thank you,

Nathan Gray

904-610-6340

**Ann O. Vickstrom**

---

**From:** joeymingione (null) <joeymingione@aol.com>  
**Sent:** Tuesday, January 24, 2023 6:22 AM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am in support of these zoning changes that I believe will allow for more housing options in areas that make sense. I also believe this should be expanded further to allow for more less-restrictive ADU options in other areas of the city as well. Thank you.

Joseph Mingione  
508 12th Avenue S

Sent from my iPhone  
Joey Mingione

## **Ann O. Vickstrom**

---

**From:** Gayle Mathews <gaylemathews@gmail.com>  
**Sent:** Wednesday, November 16, 2022 1:37 PM  
**To:** Ann O. Vickstrom  
**Subject:** Rezoning application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Ann, I received a informational postcard regarding a rezoning application. I would definitely like to rezone my house for multi family. I would like the application. Thank you.

Gail Mathews  
3433 Haines Rd N, St. Petersburg, FL 33704  
St. Petersburg, FL. 33704



## **Ann O. Vickstrom**

---

**From:** Gina Marie Foti <gina@ginaswarehouse.com>  
**Sent:** Wednesday, February 01, 2023 2:28 PM  
**To:** Ann O. Vickstrom; Derek Kilborn; Elizabeth Abernethy; Corey D. Malyszka  
**Subject:** NTM-1 zoning

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Want to thank you all for putting this together and for taking the time to listen.

I hope you guys don't hate me for being so passionate about what I believe in.

Gina

## Ann O. Vickstrom

---

**From:** Ryan Smith <smit1246@gmail.com>  
**Sent:** Wednesday, February 01, 2023 2:42 PM  
**To:** Ann O. Vickstrom  
**Subject:** NTM-1 Questions

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Ann,

I just left you a vm but wanted to send you a follow up email, as well. I am looking at a house in the uptown area (just North of Mirror Lake) and around the hospital (just west of uptown and between MLK and 16th st n). Do you know if and/or when the zoning will change to allow up to a 4 - multi-unit property in these areas?

Thanks for your help!

## Ann O. Vickstrom

---

**From:** D Kowalski <djkowalski70@outlook.com>  
**Sent:** Friday, February 03, 2023 8:51 AM  
**To:** Ann O. Vickstrom  
**Subject:** NTM-1 questions

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good morning Ann,

I attended the public hearing on Feb 1 and have just a few questions based on that meeting. I know you are very busy with this so whenever you have time to get back to me is fine. Thanks!

The information on the slides was fairly quick so I missed some points that I hoped to get more information:

1. In regarding to proposing existing front side driveways can stay when adding units, there was something about a 3 foot landscape buffer. Can you expand on what that is?
2. There was also mention of “commercial landscape requirements” for a certain number of units. Can you provide more information on this?
3. There was a mention of roof design matching adjacent homes. I did previously see the language about matching existing façades. Do you know if the matching roof requirement is strict enough to require a gable roof when the two neighbor homes have gable roofs? Or would a hip roof be allowed between two gable roofs? The reason for this question is that a hip roof is now much more desirable from a wind risk standpoint.

I’m very interested in this new zoning and have plans for multiple properties. Thanks!

Dan Kowalski

Sent from [Mail](#) for Windows

## Ann O. Vickstrom

---

**From:** Lisa Presnail <presnailphoto@gmail.com>  
**Sent:** Tuesday, November 15, 2022 2:55 PM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Ann,

I live at 2042 3rd Ave. N. only a few houses away from the proposed rezoning. I have NOT received any postcard or notification about these meetings, only just heard a few mins ago from the president of association email. It is very difficult for me to break away from work with such little notice. I am 200% against this idea of rezoning, we have enough new apartments/townhomes all around us, why encroach on a historic (and in my case Local Designated historic) district? Please tell them to build elsewhere, why ruin what is left of St. Pete's history? Plus There is super busy traffic on the road already, (20th) to add more people trying to get in and out of there is not a good idea. Please let the historic neighborhood of Kenwood breath, and shine, for all of St Pete to benefit from and prosper, not just these developers and investors.

Please advise how I can be assured of future meetings/votes/proceedings whatever so that I can attend, and have some sort of voice? I live here...literally this is on top of me, and I did not get an invitation.

Thank you,

Lisa

--

Lisa Presnail

[www.presnail.com](http://www.presnail.com)

[presnailphoto@gmail.com](mailto:presnailphoto@gmail.com)

727 871 2444

## Ann O. Vickstrom

---

**From:** Jennifer Chioma-Teolis <JENNIFERCHIOMA@Rollins.edu>  
**Sent:** Saturday, January 14, 2023 5:01 PM  
**To:** Ann O. Vickstrom  
**Subject:** Zoning changes along 30th Avenue N in Ponce De Leon

**Importance:** High

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Ann,

I'm deeply worried about the postcard I received today and the news that came with it, which is the first I'm hearing of this proposal.

This is a wretched idea. I just left a neighborhood that was re-zoned for mixed used, in part because it had become almost exclusively duplexes and all rentals. Far too many of these duplexes were owned by slum lords/investors many of them completely uninterested in maintaining the properties at all, least of all to even a decent standard. There was more crime, drug dealing, puppie mills, it was an absolute mess. All of the occupants of these multi family dwellings were renters and the population was extremely transient.

I came here to get away from that, and now the city is proposing to turn this neighborhood, for which I paid over 250K to be a part, into the same slum of investor/slum lord-owned duplexes, triplexes and fourplexes. This will not improve housing opportunities, it will simply expand opportunities for foreign investors to buy up blocks of land, tear down single-family houses and throw up cheaply built multi-family homes to rent for exorbitant amounts of money that too many people still cannot afford.

I have been in property management for 25 years, and I can tell you from all my long experience that this will not improve our situation, not for the owners who live here now and not for potential future occupants. This is a single family neighborhood, destroying that will diminish the safety of all the residents and destroy the property values of this neighborhood and I will lose what equity I do have in my home as a result, as will all of my neighbors.

I watched this exact thing happen in my last neighborhood when it was rezoned for multifamily and taken over by slum lords. A neighborhood never recovers from something like this. The neighborhoods all along 30<sup>th</sup> Avenue would be well within their rights to sue the City for the loss of safety, peaceful enjoyment, property values, and potential equity/retirement. You are asking us to live in a slum, but you're not really asking our permission, because based on this postcard, this has been in the works for a long time already and you are only just now letting

us know, when it may potentially be too late.

I'm deeply concerned about this and I wish to express my absolute objection to any such change to our zoning. You are making a grave mistake, because this will not actually help anyone, certainly not even the unhoused people who need help.

Jennifer Teolis  
jenniferchioma@rollins.edu  
2918 30<sup>th</sup> Ave N  
St Petersburg, Florida 33713  
407-947-0166

## **Ann O. Vickstrom**

---

**From:** Thomas Ley <tlnyc521@gmail.com>  
**Sent:** Monday, January 16, 2023 10:14 AM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

The maps provided on the site are unreadable. I JUST RECEIVED the the postcard of the transition to NTM-1 as the card states I will be impacted as my residence is within 300 feet. Upon trying to look at the maps it is impossible to ascertain where these housing changes are planned for in respect to my property.

Also, after just receiving this postcard many of the meetings have already taken place.

Could you please provide me a comprehensive overview of where this will be happening in respect to my residence at 2701 8th Avenue North in St. Pete. 300 feet is close by, and I purchased my home to live in a traditional neighborhood ... no one flooded with condos and townhomes.

I look forward to your reply.

Thanks in advance!

Thomas Ley

## Ann O. Vickstrom

---

**From:** Michele Angermeier <mangermeier@charter.net>  
**Sent:** Monday, January 16, 2023 10:26 AM  
**To:** Ann O. Vickstrom  
**Subject:** More is more. More is not better.

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I would be devastated if a four plex was allowed next door with an additional 8 cars minimum.  
If I wanted crowds and high rises I'd live downtown. I am sure people that bought their homes last year were not aware of meetings held in 2018.

Shame on the city and the "planners" for allowing this.

**"IT'S FINE. I'M FINE. *EVERYTHING'S FINE.*"**



## Ann O. Vickstrom

---

**From:** Greg Tappan <gregtappan70@gmail.com>  
**Sent:** Monday, January 16, 2023 11:00 AM  
**To:** Ann O. Vickstrom  
**Subject:** City Application ZM-15

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Ann. My name is Greg Tappan and my address is 3650 Foster Hill Dr. N., 33704. We received a postcard last week about a proposed rezoning, and I'd like to get more information about this. I tried to read through the info on the city site and tried to look at the map, but the map is very hard to understand and I'm confused about what this is. Is this a specific application by a property owner to build multiple units on their property, or a broader rezoning change, or something else? As residents of Allendale, we have seen a disturbing number of out of character development, carving up the large, estate lots into smaller parcels and harming the character of what made our neighborhood so great in the first place. So anything that proposes multiple dwelling units replacing single family homes within 175 feet of our property is concerning.

If you could provide more information on this project, and the property addresses requesting to be rezoned for multiple dwelling units it would help us understand this better. Thank you in advance for your help.

## Ann O. Vickstrom

---

**From:** Michael Winterbottom <michaelwinterbottom1@gmail.com>  
**Sent:** Monday, January 16, 2023 1:24 PM  
**To:** Ann O. Vickstrom  
**Subject:** Zoning amendment

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I received alarming news in the mail Saturday about rezoning ( I assume the property right across the street from me at 4820 17th Ave N.) It pretty much ruined my weekend. My family has owned our house since 1969, so the thought of a three or four plex housing unit with three or four or who knows how many families living on this property is egregious. Two two story house like I have seen going up on other recent properties would at least be more acceptable, but I would prefer this be a single house lot. We live in a quite neighborhood and this monstrosity would have absolutely no place in this neighborhood. The owner of that house along with the zoning board who are making this decision for the rest of us don't live here and they're forcing this on our neighborhood. How are people who are not going to reside on said property aloud to ruin other people's peace and tranquility. Furthermore I find it unsettling that these meetings to decide this will be during regular working hours when you know full well that people probably won't be able to attend giving carte blanche to the opposition to walk away with everything they want. When does this over development stop, are water sewage system is over taxed and yet with absolutely no regard to that you keep approving more building. This should have been rejected at the get go, but apparently we have absolutely 0 rules now on urban development.

## Ann O. Vickstrom

---

**From:** Grzelka, Heather <heather@grzelka.us>  
**Sent:** Tuesday, January 17, 2023 8:06 AM  
**To:** Ann O. Vickstrom  
**Subject:** City Application for Zoning Map Amendment

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Ann,

We'd like to register for the upcoming public hearings regarding city application ZM-15. For those in our community who can't take off work to oppose this, what options do they have?

With appreciation,  
Heather Grzelka

## Ann O. Vickstrom

---

**From:** Michele Angermeier <mangermeier@charter.net>  
**Sent:** Tuesday, January 17, 2023 3:31 PM  
**To:** Ann O. Vickstrom  
**Subject:** RE: More is more. More is not better.

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I don't want to be sitting in my backyard having eyes looking at me from the two story "apartment" you want to build next door. Also explain to everyone how evacuation would work during a hurricane. How will our antiquated sewer system handle this. How will our roads deal with the added congestion and added road rage.

Just say no. Enough is enough.

*"IT'S FINE. I'M FINE. EVERYTHING'S FINE."*

---

**From:** [Ann O. Vickstrom](#)  
**Sent:** Tuesday, January 17, 2023 2:48 PM  
**To:** '[Michele Angermeier](#)'  
**Subject:** RE: More is more. More is not better.

Ms. Angermeier,

Thank you for submitting your comments to the City. This email is to confirm our receipt of your comments. All comments will be provided to City Council as part of the NTM-1 rezoning report.

Respectfully,

Ann Vickstrom, AICP, RLA#0001122  
Planner II, Urban Planning and Historic Preservation  
City of St. Petersburg  
(727)892-5807  
[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)

Please note all emails are subject to public records law.

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**From:** Michele Angermeier <mangermeier@charter.net>  
**Sent:** Monday, January 16, 2023 10:26 AM  
**To:** Ann O. Vickstrom <[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)>  
**Subject:** More is more. More is not better.

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I would be devastated if a four plex was allowed next door with an additional 8 cars minimum.

If I wanted crowds and high rises I'd live downtown. I am sure people that bought their homes last year were not aware of meetings held in 2018.

Shame on the city and the "planners" for allowing this.

**"IT'S FINE. I'M FINE. *EVERYTHING'S FINE.*"**

## Ann O. Vickstrom

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**From:** martohued@gmail.com  
**Sent:** Wednesday, January 18, 2023 5:31 PM  
**To:** Ann O. Vickstrom; Derek Kilborn; Lisset G. Hanewicz  
**Subject:** Objection to RE City Application ZM-15 proposing NTM-1 Rezoning -Please submit into the public hearing record  
**Attachments:** NTM\_Map\_PRR.pdf; NTM-1 Proposed Rezoning Map -enlargement.JPG

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Councilmember District 4 Lisset Hanewicz, Mr. Kilborn & Ms. Vickstrom:

My name is Deborah Martohue and I live in District 4 at 1036 23<sup>rd</sup> Avenue N. I have owned my home for over 5 years. I have just been made aware by my next door neighbor located at 1042 23<sup>rd</sup> Avenue North of the City's Notice of Public Hearings re the captioned matter. Interestingly, I have neither received this mailed notice nor any notices /invitations for any of the prior community outreach meetings that I see listed when I go online to review the information posted by the City. As a resident that is literally across the alley from the properties proposed to be rezoned, I would have to give the City's community outreach a failing grade. As Mr. Kilborn may recall, I am an experienced land use, zoning and appellate attorney since 1996, a certified planner since 1998, a former elected official (2004-2006) and an appointed member of the Pinellas County Planning Commission (2004-2006) representing the municipalities of St. Pete Beach, Treasure Island and Madeira Beach. I was also Co-Chair of the Traditional Neighborhood Committee in 2002-2003 during the Code rewrite that helped craft the current land development regulations for the Traditional Neighborhood corridor focused on 4<sup>th</sup> Street North. I not only have vast experience in land use and zoning matters including drafting local land use regulations, I have vast experience in community outreach above and beyond the average resident and thus, I believe I am qualified to opine on the lack of actual outreach in this matter as well as the substance of the planning proposal.

If I had been notified prior to today by my neighbor, I would have voiced my concerns on the front end of this City-initiated rezoning effort rather than the tail end when changes are more difficult to achieve. Anyone who knows me and knows my law practice also knows that no one would classify me as a NIMBY. I am also not pro-development at all costs. I believe in updating development regulations to address current conditions and needs, including affordable housing initiatives. I can even understand as a planner that on paper, rezoning the parcels along major roadways such as 5<sup>th</sup>, 9<sup>th</sup>, 22<sup>nd</sup>, 28<sup>th</sup> etc. as transitional zoning to the single family neighborhoods located behind those roadways may appear appealing however, it does not work from an operational or functional standpoint on roads that do not have access driveways, on-street parking and are accessed completely from a rear alleyway such as the lots fronting 22<sup>nd</sup> Avenue N for example.

I live full-time in a single family home with a driveway and garage that has rear alley access shared with those parcels fronting 22<sup>nd</sup> Avenue N. Since those houses fronting 22<sup>nd</sup> Ave N do not have driveway access or on-street parking along 22<sup>nd</sup> Avenue N., the alleyway is already a parking obstacle course with many people parking parallel to the alleyway either because they do not have rear garages and/or driveways or they have too many cars. Currently, they are single family homes with a couple of those houses between 10<sup>th</sup> and 11<sup>th</sup> Street having garage apartments. I invite staff and my District Commissioner to visit the alley between 22<sup>nd</sup> Ave N and 23<sup>rd</sup> Avenue N and between 10<sup>th</sup> and 11<sup>th</sup> Streets to see for yourself. If you call or email me, I am happy to accompany any of you on a site visit so I can explain to you what the issues are currently and why the proposed rezoning would only serve to exacerbate the problem.

I routinely have issues getting in and out of my driveway and garage because of how the people park in the house behind me located at 1035 22<sup>nd</sup> Ave N. The landlord owner recently sold the property to an investor and the house is

being remodeled. Their heavy equipment destroyed my historic brick driveway late last week because they can't maneuver their equipment to unload a portable dumpster within the confines of this narrow alleyway located between our lots. How exactly do you think reasonable access in and out of my garage and/or driveway will work if up to 4 units are allowed on each of these lots behind me if it is problematic now with a single family home located across the alley from me? Parking will be at least 3-4 times worse not to mention the damage to the historic brick alleyway and abutting properties that will happen inevitably with construction and delivery trucks and equipment.

Since these units will not have access from 22<sup>nd</sup> Ave N., the traffic trips will quadruple, at least, on this narrow alleyway. When any one of these 12 lots located between 10<sup>th</sup> and 11<sup>th</sup> street are under construction, the heavy equipment, i.e., construction supply delivery trucks, dumpster containers, concrete mixers and the like will not only block the alleyway from use by anyone living between 10<sup>th</sup> and 11<sup>th</sup> streets on 22<sup>nd</sup> Avenue and 23<sup>rd</sup> Avenue N (it's not a 2-way street), it will disrupt garbage/recycling and other delivery services and inevitably damage abutting properties across the alley because of inadequate turnaround width. Yes, it happens as I just experienced last week and I have photos to support my allegation. Has the City done a traffic study to quantify the increase in traffic trips along this alleyway or any other alleyway where the fronting roadway does not provide on-street parking or driveway access and the only access option is use of the alley? These access and traffic issues must be considered.

How many more large City Dumpster bins do you think the City can fit along the alley? As it is, we have pick up twice per week with one dumpster servicing 4 single family homes that is filled or overfilled for each garbage pick-up day. Is the City going to pick up our garbage daily to accommodate a 4-fold increase in Units located behind my home between 10<sup>th</sup> and 11<sup>th</sup> Streets?

These are small City lots 50' wide and 128' deep. How will 4 units with at least 4 parking spaces fit on this size lot and meet all setback and pervious area requirements? If you do not require 4 parking spaces on-site respecting rear yard setbacks, there will still be at least one car per unit and they will park parallel alongside the alley making parking, access and travel within the alley that much worse than it already is.

We already have a stormwater runoff issue in the City with flash flooding on streets when we have heavy rains from stormwater runoff so I hope an increase of impervious area is not going to be allowed to accommodate the quadrupling of density.

Again, I am not opposed to affordable housing initiatives but I am strongly opposed to quadrupling density behind single family homes arising from inadequate public safety access & parking, inadequate facility capacity and services and an unreasonable increase in traffic on an alleyway not designed for such traffic increase as detailed above.

Please confirm receipt and acknowledge that my written objection will be made part of the hearing agenda packages and included as part of the public hearing record for every single upcoming public hearing on this matter. I will plan on attending one or more of the scheduled hearings as my work schedule allows noting that these hearings except for the last one scheduled at 5pm, all occur during normal business hours. This schedule of hearings in and of itself does not encourage attendance by interested and affected persons and constitutes an illusory effort to allow reasonable input by residents.

My contact information is below and I welcome any one of you or all of you to reach out to discuss or have me accompany you on a site visit. I look forward to hearing from you. Thank you in advance for your time and consideration.

Sincerely,  
Deborah Martohue  
1036 23<sup>rd</sup> Avenue N.  
St. Petersburg, FL 33704  
Mobile: 727.460.8431  
Office: 727.256.1211  
[martohued@gmail.com](mailto:martohued@gmail.com)





## Ann O. Vickstrom

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**From:** john potts <postalpotts@yahoo.com>  
**Sent:** Thursday, January 19, 2023 7:42 PM  
**To:** Ann O. Vickstrom  
**Subject:** Re: city application zm-15  
**Attachments:** Capture.PNG

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you for the clarification on the map. My question is, will these properties still be allowed to redevelop given that we have local historic designation status? The whole purpose of our historic designation was to preserve older homes. Encouraging redevelopment, along with a higher density, is in direct contrast of that purpose.

Thank you for your time,

John Potts

On Monday, January 16, 2023 at 04:01:13 PM EST, Ann O. Vickstrom <ann.vickstrom@stpete.org> wrote:

John,


Thank you for your inquiry. The Future Major Street is 28<sup>th</sup> Street. I have provided a screen shot of the NTM-1 map showing your property and surrounding properties that are proposed for rezoning below:



The properties in red are the properties that are proposed for the NTM-1 zoning following the NTM-1 design and development guidelines. The NTM-1 district is proposed with many of the same building characteristics as the Neighborhood Traditional (NT). This zoning is proposed to allow property owners to add units onto their existing house while providing appropriate parking on site.

The screen shot below is of the resource page and I have provided an arrow showing which maps to go to in order to find this map.

## Resources

- [St. Petersburg Comprehensive Plan](#)
  - St. Petersburg City Code, Chapter 16, Land Development F
    - [Matrix: Use Permissions, Parking, and Zoning](#)
    - [Neighborhood Traditional Mixed Residential \(NTM-1\)](#)
    - [Neighborhood Traditional \(NT\)](#)
    - [Neighborhood Suburban \(NS\)](#)
  - Map Collection
    - [NTM-1 PDF Map](#)
    - [NTM-1 Interactive Map](#)
    - [Interactive Zoning and Future Land Use Map](#)
- 

Please let me know if you have additional questions.

Thank you for contacting our office.

Respectfully,

Ann Vickstrom, AICP, RLA#0001122

Planner II, Urban Planning and Historic Preservation

City of St. Petersburg

(727)892-5807

[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)

Please note all emails are subject to public records law.

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**From:** john potts <postalpotts@yahoo.com>  
**Sent:** Saturday, January 14, 2023 3:58 PM  
**To:** Ann O. Vickstrom <Ann.Vickstrom@stpete.org>  
**Subject:** city application zm-15

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello! We received a postcard about the zoning map amendment which is within 300 feet of our property. However, I cannot find any information on the listed link to tell me which street is the Future Major Street and which properties will have zoning changes if the amendment is implemented. My address is 2836 7th Ave N. Please clarify or direct me to the actual page which shows the changes.

Thank you for your help,

John Potts

[postalpotts@yahoo.com](mailto:postalpotts@yahoo.com)

## Ann O. Vickstrom

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**From:** martohued@gmail.com  
**Sent:** Friday, January 20, 2023 11:26 AM  
**To:** Derek Kilborn; Ann O. Vickstrom; Lisset G. Hanewicz  
**Subject:** RE: Objection to RE City Application ZM-15 proposing NTM-1 Rezoning -Please submit into the public hearing record

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you for confirming receipt.

I reiterate my offer to accompany one or all of you on a walking tour of this alley. I can't envision that this alley could accommodate all of the additional traffic generated by quadruple the number of residents as well as all of the increased deliveries (i.e., FEDEX, UPS, Amazon and all the other delivery services people avail themselves of these days) which require stopping the delivery vehicle in the alley, getting out and delivering the package, plus all of the visitors to these new units. Where will they park? It's very different when you have a two-way street with on-street parking in front of your home. These lots do not have that which dumps 100% of parking, traffic and deliveries into the alleyway. The alleyway is already at capacity with traffic and parking. Add-on all of the dust, noise and fumes from all of this additional traffic and deliveries not to mention the issues created by construction. I honestly cannot wrap my head around the full scope of the adverse public safety, traffic and nuisance impacts that will be caused to all of the residents abutting this alleyway from this proposal. I am still trying to process all of the impacts. I respect this City and its staff but with all due respect, I do not think this proposal was thought through as to how the alleyway will actually function or should I say not function well, with this proposed quadrupling of density along 22<sup>nd</sup> Ave N.

I implore you to reconsider your proposal at least for any lots that abut roadways that do not have driveway access or on-street parking.

Sincerely,

Deborah Martohue  
1036 23<sup>rd</sup> Avenue N.  
St. Petersburg, FL 33704  
727.460.8431  
[martohued@gmail.com](mailto:martohued@gmail.com)

---

**From:** Derek Kilborn <Derek.Kilborn@stpete.org>  
**Sent:** Friday, January 20, 2023 6:27 AM  
**To:** 'martohued@gmail.com' <martohued@gmail.com>; Ann O. Vickstrom <Ann.Vickstrom@stpete.org>; Lisset G. Hanewicz <Lisset.Hanewicz@stpete.org>  
**Subject:** RE: Objection to RE City Application ZM-15 proposing NTM-1 Rezoning -Please submit into the public hearing record

Deborah Martohue:

Thank you for your interest in this important application. I am writing to confirm receipt of your written objection and acknowledge that your email will be included as a public comment attachment to the staff report for each public hearing.

Respectfully,

**Derek Kilborn, Manager**

Urban Planning and Historic Preservation Division

Planning and Development Services Department

One 4th Street North, St. Petersburg, Fla. 33701

[Derek.Kilborn@stpete.org](mailto:Derek.Kilborn@stpete.org)

(727) 893-7872



---

**From:** [martohued@gmail.com](mailto:martohued@gmail.com) <[martohued@gmail.com](mailto:martohued@gmail.com)>

**Sent:** Wednesday, January 18, 2023 5:31 PM

**To:** Ann O. Vickstrom <[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)>; Derek Kilborn <[Derek.Kilborn@stpete.org](mailto:Derek.Kilborn@stpete.org)>; Lisset G. Hanewicz <[Lisset.Hanewicz@stpete.org](mailto:Lisset.Hanewicz@stpete.org)>

**Subject:** Objection to RE City Application ZM-15 proposing NTM-1 Rezoning -Please submit into the public hearing record

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Councilmember District 4 Lisset Hanewicz, Mr. Kilborn & Ms. Vickstrom:

My name is Deborah Martohue and I live in District 4 at 1036 23<sup>rd</sup> Avenue N. I have owned my home for over 5 years. I have just been made aware by my next door neighbor located at 1042 23<sup>rd</sup> Avenue North of the City's Notice of Public Hearings re the captioned matter. Interestingly, I have neither received this mailed notice nor any notices /invitations for any of the prior community outreach meetings that I see listed when I go online to review the information posted by the City. As a resident that is literally across the alley from the properties proposed to be rezoned, I would have to give the City's community outreach a failing grade. As Mr. Kilborn may recall, I am an experienced land use, zoning and appellate attorney since 1996, a certified planner since 1998, a former elected official (2004-2006) and an appointed member of the Pinellas County Planning Commission (2004-2006) representing the municipalities of St. Pete Beach, Treasure Island and Madeira Beach. I was also Co-Chair of the Traditional Neighborhood Committee in 2002-2003 during the Code rewrite that helped craft the current land development regulations for the Traditional Neighborhood corridor focused on 4<sup>th</sup> Street North. I not only have vast experience in land use and zoning matters including drafting local land use regulations, I have vast experience in community outreach above and beyond the average resident and thus, I believe I am qualified to opine on the lack of actual outreach in this matter as well as the substance of the planning proposal.

If I had been notified prior to today by my neighbor, I would have voiced my concerns on the front end of this City-initiated rezoning effort rather than the tail end when changes are more difficult to achieve. Anyone who knows me and knows my law practice also knows that no one would classify me as a NIMBY. I am also not pro-development at all costs. I believe in updating development regulations to address current conditions and needs, including affordable housing initiatives. I can even understand as a planner that on paper, rezoning the parcels along major roadways such as 5<sup>th</sup>, 9<sup>th</sup>,

22<sup>nd</sup>, 28<sup>th</sup> etc. as transitional zoning to the single family neighborhoods located behind those roadways may appear appealing however, it does not work from an operational or functional standpoint on roads that do not have access driveways, on-street parking and are accessed completely from a rear alleyway such as the lots fronting 22<sup>nd</sup> Avenue N for example.

I live full-time in a single family home with a driveway and garage that has rear alley access shared with those parcels fronting 22<sup>nd</sup> Avenue N. Since those houses fronting 22<sup>nd</sup> Ave N do not have driveway access or on-street parking along 22<sup>nd</sup> Avenue N., the alleyway is already a parking obstacle course with many people parking parallel to the alleyway either because they do not have rear garages and/or driveways or they have too many cars. Currently, they are single family homes with a couple of those houses between 10<sup>th</sup> and 11<sup>th</sup> Street having garage apartments. I invite staff and my District Commissioner to visit the alley between 22<sup>nd</sup> Ave N and 23<sup>rd</sup> Avenue N and between 10<sup>th</sup> and 11<sup>th</sup> Streets to see for yourself. If you call or email me, I am happy to accompany any of you on a site visit so I can explain to you what the issues are currently and why the proposed rezoning would only serve to exacerbate the problem.

I routinely have issues getting in and out of my driveway and garage because of how the people park in the house behind me located at 1035 22<sup>nd</sup> Ave N. The landlord owner recently sold the property to an investor and the house is being remodeled. Their heavy equipment destroyed my historic brick driveway late last week because they can't maneuver their equipment to unload a portable dumpster within the confines of this narrow alleyway located between our lots. How exactly do you think reasonable access in and out of my garage and/or driveway will work if up to 4 units are allowed on each of these lots behind me if it is problematic now with a single family home located across the alley from me? Parking will be at least 3-4 times worse not to mention the damage to the historic brick alleyway and abutting properties that will happen inevitably with construction and delivery trucks and equipment.

Since these units will not have access from 22<sup>nd</sup> Ave N., the traffic trips will quadruple, at least, on this narrow alleyway. When any one of these 12 lots located between 10<sup>th</sup> and 11<sup>th</sup> street are under construction, the heavy equipment, i.e., construction supply delivery trucks, dumpster containers, concrete mixers and the like will not only block the alleyway from use by anyone living between 10<sup>th</sup> and 11<sup>th</sup> streets on 22<sup>nd</sup> Avenue and 23<sup>rd</sup> Avenue N (it's not a 2-way street), it will disrupt garbage/recycling and other delivery services and inevitably damage abutting properties across the alley because of inadequate turnaround width. Yes, it happens as I just experienced last week and I have photos to support my allegation. Has the City done a traffic study to quantify the increase in traffic trips along this alleyway or any other alleyway where the fronting roadway does not provide on-street parking or driveway access and the only access option is use of the alley? These access and traffic issues must be considered.

How many more large City Dumpster bins do you think the City can fit along the alley? As it is, we have pick up twice per week with one dumpster servicing 4 single family homes that is filled or overfilled for each garbage pick-up day. Is the City going to pick up our garbage daily to accommodate a 4-fold increase in Units located behind my home between 10<sup>th</sup> and 11<sup>th</sup> Streets?

These are small City lots 50' wide and 128' deep. How will 4 units with at least 4 parking spaces fit on this size lot and meet all setback and pervious area requirements? If you do not require 4 parking spaces on-site respecting rear yard setbacks, there will still be at least one car per unit and they will park parallel alongside the alley making parking, access and travel within the alley that much worse than it already is.

We already have a stormwater runoff issue in the City with flash flooding on streets when we have heavy rains from stormwater runoff so I hope an increase of impervious area is not going to be allowed to accommodate the quadrupling of density.

Again, I am not opposed to affordable housing initiatives but I am strongly opposed to quadrupling density behind single family homes arising from inadequate public safety access & parking, inadequate facility capacity and services and an unreasonable increase in traffic on an alleyway not designed for such traffic increase as detailed above.

Please confirm receipt and acknowledge that my written objection will be made part of the hearing agenda packages and included as part of the public hearing record for every single upcoming public hearing on this matter. I will plan on attending one or more of the scheduled hearings as my work schedule allows noting that these hearings except for the last one scheduled at 5pm, all occur during normal business hours. This schedule of hearings in and of itself does not encourage attendance by interested and affected persons and constitutes an illusory effort to allow reasonable input by residents.

My contact information is below and I welcome any one of you or all of you to reach out to discuss or have me accompany you on a site visit. I look forward to hearing from you. Thank you in advance for your time and consideration.

Sincerely,  
Deborah Martohue  
1036 23<sup>rd</sup> Avenue N.  
St. Petersburg, FL 33704  
Mobile: 727.460.8431  
Office: 727.256.1211  
[martohued@gmail.com](mailto:martohued@gmail.com)



## Ann O. Vickstrom

---

**From:** Toby Duffield <tduffield@tierraeng.com>  
**Sent:** Friday, January 20, 2023 12:50 PM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I just want to put my 2 cents in on the CITY APPLICATION ZM-15. The parking in this neighborhood has already been a huge issue for residents on Dartmouth Ave North because you can only park on one side of the street. I would also like to point out that our alley where our single car driveways connect is basically dirt which is pointless to park a car back there unless you want to wash your car every day. We are also not allowed to put driveways in the front of our homes unless you are grandfathered in. If you allow these houses to be bought and converted to multi-dwellings, It's going to become more of a nightmare for homeowner's and their families that are already searching for parking.

**Toby Duffield**

Senior Geophysical Supervisor

**TIERRA, INC.**

7351 Temple Terrace Highway | Tampa, Florida 33637

T 813.989.1354 | F 813.989.1355 | C 813.992.1330

[www.tierraeng.com](http://www.tierraeng.com) | [tduffield@tierraeng.com](mailto:tduffield@tierraeng.com)

## Ann O. Vickstrom

---

**Subject:** FW: ZM-15: NTM-1 Rezoning Application

---

**From:** Thomas Ley <tlnyc521@gmail.com>  
**Sent:** Friday, January 20, 2023 2:07 PM  
**To:** Ann O. Vickstrom <Ann.Vickstrom@stpete.org>  
**Subject:** Re: ZM-15: NTM-1 Rezoning Application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Ann -

Thanks for sending this along to me. I have to admit that I'm not very pleased about it, and am surprised it can happen in the historic kenwood area. I would not have purchased my home in a neighborhood that would/could be rezoned into this type of a format.

I know my neighbors are not pleased either. I am rallying support to attend the upcoming meeting to protest these changes to our charming neighborhood.

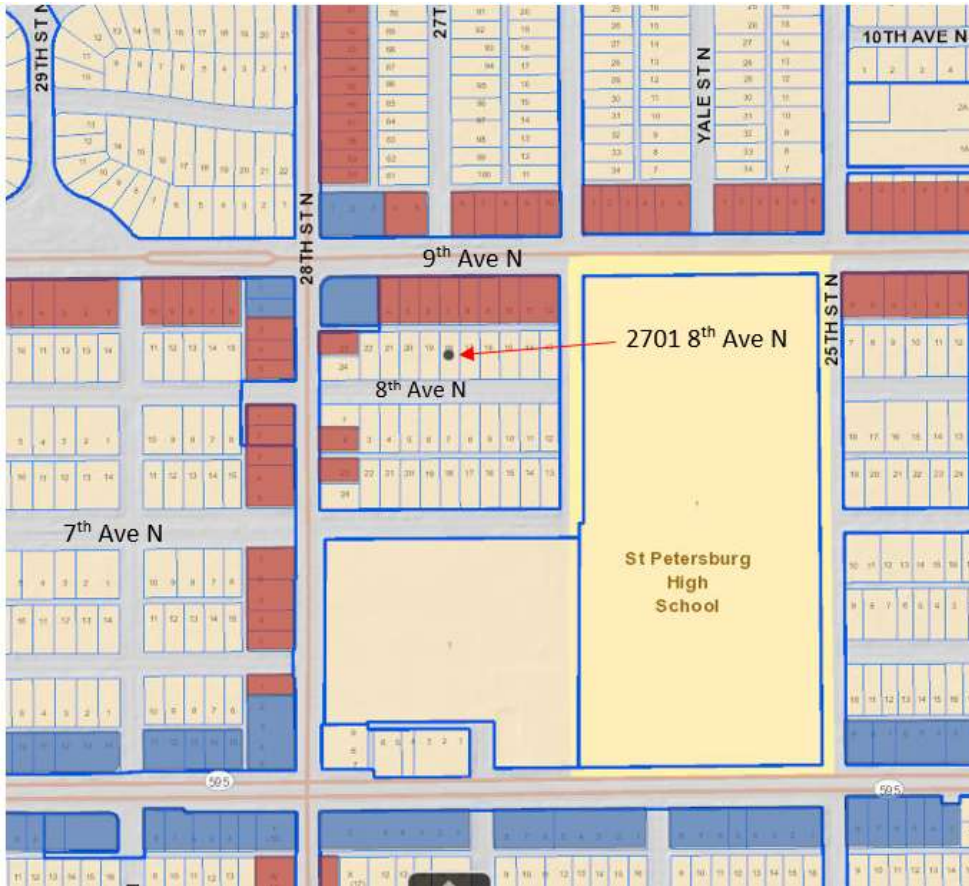
Regards,

Tom

On Tue, Jan 17, 2023 at 2:41 PM Ann O. Vickstrom <[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)> wrote:

Mr. Ley,

Thank you for contacting the City with your concerns about the proposed NTM-1 rezoning. I have provided a screen shot of the NTM-1 interactive map on the webpage that shows the location of your house in relation to the surrounding proposed NTM-1 properties. The red properties are proposed for the NTM-1 zoning.



The NTM-1 zoning has several locational criteria.

- within 175-feet of the centerline of a designated *Future Major Street*
- properties to be adjacent to a public alley.
- retains direct connectivity to one or more adjoining *Future Major Streets*; and
- located outside of the designated Coastal High Hazard Area ("CHHA").

The NTM-1 is a traditional zoning where the individual homeowners will have the ability to construct additional units on their property, added to their existing homes. The NT zoning district currently allows two units (the primary unit and an accessory dwelling unit (ADU)). The NTM-1 zoning allows the owner to add up to 2 more smaller units, meeting all locational, design and dimensional restrictions. Building requirements are much like the NT zoning district by limiting the building height to 2 stories and a building width of 40 feet. Buildings are still required to have front porches and parking must be on-site and accessed from the alley.

Please let me know if you have additional questions. We appreciate that you have contacted our office to learn more about this proposal. All public comments will be provided to the City Council as part of the report.

Respectfully,

Ann Vickstrom, AICP, RLA#0001122

Planner II, Urban Planning and Historic Preservation

City of St. Petersburg

(727)892-5807

[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)

Please note all emails are subject to public records law.

-----Original Message-----

From: Thomas Ley <[tl NYC521@gmail.com](mailto:tl NYC521@gmail.com)>

Sent: Monday, January 16, 2023 10:14 AM

To: Ann O. Vickstrom <[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)>

Subject: ZM-15: NTM-1 Rezoning Application

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

The maps provided on the site are unreadable. I JUST RECEIVED the the postcard of the transition to NTM-1 as the card states I will be impacted as my residence is within 300 feet. Upon trying to look at the maps it is impossible to ascertain where these housing changes are planned for in respect to my property.

Also, after just receiving this postcard many of the meetings have already taken place.

Could you please provide me a comprehensive overview of where this will be happening in respect to my residence at 2701 8th Avenue North in St. Pete. 300 feet is close by, and I purchased my home to live in a traditional neighborhood ... no one flooded with condos and townhomes.

I look forward to your reply.

Thanks in advance!

Thomas Ley

## **Ann O. Vickstrom**

---

**From:** Kate Zamboni <kate.zamboni@yahoo.com>  
**Sent:** Sunday, January 22, 2023 8:42 PM  
**To:** Ann O. Vickstrom  
**Cc:** Norman Zamboni  
**Subject:** ZM-15: NTM-1 Rezoning Application

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I live at 1390 42nd Ave N and am writing to express my opposition to the planned amendment that would intensify the residential density along Haines Avenue between 38th Ave N and 16th Street. I use that stretch of road to ride my bike to access the new bike lanes on MLK because MLK north of 38th Ave is too busy for bike traffic.

The plan will make this currently relatively quiet section Haines Road a very busy road. As it is, traffic is increasing on Haines Road with through traffic traveling at too high a rate of speed for this residential street. Moreover, there is little room for street parking on this stretch of Haines Road and increasing the number of dwelling units is certain to create a serious parking problem that will lead to unnecessary congestion with cars parked on the street. Please remove the section of Haines Road between 38th Ave N and 16th Street from the amendment.

Thank you,  
Kate Zamboni

Sent from my iPhone.

## Ann O. Vickstrom

---

**From:** Norm Zamboni <normzamboni@hotmail.com>  
**Sent:** Monday, January 23, 2023 8:18 AM  
**To:** Kate Zamboni  
**Cc:** Ann O. Vickstrom  
**Subject:** Re: ZM-15: NTM-1 Rezoning Application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Is it possible to read the rezoning application? I don't have time to read it. But I'm kind of curious as to what it says.

Get [BlueMail for Android](#)

On Jan 22, 2023, at 8:42 PM, Kate Zamboni <[kate.zamboni@yahoo.com](mailto:kate.zamboni@yahoo.com)> wrote:

I live at 1390 42nd Ave N and am writing to express my opposition to the planned amendment that would intensify the residential density along Haines Avenue between 38th Ave N and 16th Street. I use that stretch of road to ride my bike to access the new bike lanes on MLK because MLK north of 38th Ave is too busy for bike traffic.

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Thank you,

Kate Zamboni

Sent from my iPhone.

## Ann O. Vickstrom

---

**From:** Norm Zamboni <normzamboni@hotmail.com>  
**Sent:** Monday, January 23, 2023 8:18 AM  
**To:** Kate Zamboni  
**Cc:** Ann O. Vickstrom  
**Subject:** Re: ZM-15: NTM-1 Rezoning Application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Is it possible to read the rezoning application? I don't have time to read it. But I'm kind of curious as to what it says.

Get [BlueMail for Android](#)

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The plan will make this currently relatively quiet section Haines Road a very busy road. As it is, traffic is increasing on Haines Road with through traffic traveling at too high a rate of speed for this residential street. Moreover, there is little room for street parking on this stretch of Haines Road and increasing the number of dwelling units is certain to create a serious parking problem that will lead to unnecessary congestion with cars parked on the street. Please remove the section of Haines Road between 38th Ave N and 16th Street from the amendment.

Thank you,

Kate Zamboni

Sent from my iPhone.



## Ann O. Vickstrom

---

**From:** Jamie Dickerson <alwaysjld@mac.com>  
**Sent:** Monday, January 23, 2023 11:32 AM  
**To:** Ann O. Vickstrom  
**Subject:** Rezoning information

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Morning Ms. Vickstrom.

I understanding there will be a series of public meetings to address rezoning of my neighborhood. Would you be kind enough to send me

rezoning information in my neighborhood Allendale Oaks

Opponent Registration information, restrictions, application and processes

Thank you for your time.

Jamie Dickerson  
861 42nd Ave North  
St. Petersburg, Florida 33703  
305 336 4247

## Ann O. Vickstrom

---

**From:** Goran I <goran63@hotmail.com>  
**Sent:** Monday, January 23, 2023 1:23 PM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I would like to express my profound dissatisfaction with the upcoming zoning map amendment initiative. I realize that this public comment along with the upcoming and past public hearings are merely a formality to somehow slap some form of legality on the further destruction of St. Petersburg. Adding density in an already overcrowded city does not make any sense for its residents, but it does make perfect sense for "developers" and for those whose pockets they line. As a resident I do not want more congestion, more traffic, more crime. I have never seen this town so congested. It takes twice as long to get anywhere compared to just a few years ago. And no, the solution is not to give up my car and ride those scooters or some unicorn..... I am being asked not to use my washer during peak storm events so that it does not overburden the sewer system..... It will inevitably get worse following your grand plan for zoning amendment. We do not need any more people in this city! Enough is enough!

Goran Ivanov  
St. Petersburg FL

## Ann O. Vickstrom

---

**From:** Barbara Nicolaisen <rebdux0927@gmail.com>  
**Sent:** Tuesday, January 24, 2023 10:15 AM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am NOT in favor of the proposed "NTM-1 Map Amendment."

Barbara Nicolaisen

**Ann O. Vickstrom**

---

**From:** Elizabeth Abernethy  
**Sent:** Tuesday, January 24, 2023 7:04 PM  
**To:** Ann O. Vickstrom; Derek Kilborn  
**Subject:** Fwd: ALERT: Planned City Rezoning Affecting OUR Neighborhood

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

Sent from my iPhone

Begin forwarded message:

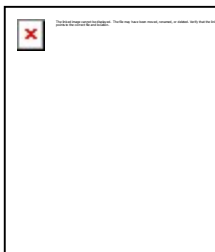
**From:** "Susan P. Ajoc" <susan.ajoc@stpete.org>  
**Date:** January 24, 2023 at 5:37:31 PM EST  
**To:** Elizabeth Abernethy <Elizabeth.Abernethy@stpete.org>, Derek Kilborn <derek.kilborn@stpete.org>  
**Subject:** FW: ALERT: Planned City Rezoning Affecting OUR Neighborhood

FYI – not sure if they reached out to you

---

**From:** Carol Gruszka <noreply@classcreator.net>  
**Sent:** Tuesday, January 24, 2023 3:05 PM  
**To:** Susan P. Ajoc <Susan.Ajoc@stpete.org>  
**Subject:** ALERT: Planned City Rezoning Affecting OUR Neighborhood

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.



## Lake Pasadena Estates Neighborhood Association

The LPENA Board of Directors (BOD) met last evening and learned that the City of St. Petersburg plans to adopt zoning map changes that would directly affect our Lake Pasadena Estates neighborhood on all four sides,

primarily north and south (south side of 5th Ave. N and north side of 1st Ave. N, respectively), but also on the east and west (58th St. and 66th St.). Details on the [NTM-1 Map Amendment](#) may be read online by clicking on the underlined link, but basically the amended map zoning would allow multi-family dwellings (up to "quadplexes") to be built (or converted) on single properties up to 150 feet into our neighborhood on every side. These border areas would change from NT (Neighborhood Traditional) to NTM (Neighborhood Traditional Mixed) to allow greater population density along major corridor streets (bus routes). Although these planning meetings are well underway, three upcoming public meetings are rapidly approaching and warrant our/your attention.

**Community Planning & Preservation Commission:** February 14, 2023, 2 p.m. at City Hall Chamber, 175 5<sup>th</sup> St. N.

**City Council First Reading & Public Hearing:** March 2, 2023, 3 p.m. at City Hall Chamber, 175 5<sup>th</sup> St. N.

**City Council Second Reading & Adoption Hearing:** March 23, 2023, 5:01 p.m. at City Hall Chamber, 175 5<sup>th</sup> St. N.

Two LPENA directors, Niel Allen and Lorraine Armstrong, have been designated by the BOD to attend and speak (if allowed) at these meetings, which require a 10-day-advance registration to attend and speak at these scheduled meetings. We are NOT limited to their speaking, however, since you, too, may want to register to attend and speak your concerns.

Even more alarmingly, the above-planned zoning changes are, in the future, planned to be extended an additional 200 feet into our neighborhoods after these current changes take place.

We understand the need for affordable housing, but it was noted at last night's BOD meeting that the city currently has 14,000 properties already zoned and available for building such multi-family dwellings. We also know that construction of a 100+ unit development is already underway on the south side of Central Avenue between 60th and 61st Streets and that there is another potentially planned development of more than 1,000 units on the old Raytheon Technologies property located at 17th Ave. and 72nd St.).

<https://www.classcreator.com/St-Petersburg-Florida-Lake-Pasadena-Estates-2019/>



This message has been sent to you from [carol@prismeffects.com](mailto:carol@prismeffects.com). You cannot reply directly. [Click here if you wish to start a new email to carol@prismeffects.com](#).

To discontinue these email notifications [unsubscribe](#).



## Ann O. Vickstrom

---

**From:** Lane <lanedriscoll@gmail.com>  
**Sent:** Wednesday, January 25, 2023 4:49 PM  
**To:** Ann O. Vickstrom  
**Subject:** Zoning map amendment and designated neighborhood associations

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I recently received a postcard advising me of public hearings about changing density levels in my neighborhood, because I live within 300 feet of a FUTURE MAJOR STREET. Though I've owned my home for over 15 years as a landlord, I recently moved to make it my full-time home. This postcard is the 1st notice I've received about a process that has been in the works for years. I'm on board with allowing more housing options, like ADUs and garage apartments in traditional SFR areas, but I'm not looking to have massive rental/condo complexes/towers spring up along 22nd Avenue South.

I went on the website given on the postcard and couldn't find a definition of what types/sizes of development/buildings will be allowed under NTM-1. I also couldn't find a definition of Future Major Street (FMS), nor which current streets are going to be turned into a FMS. Currently, 22nd Ave South is already a major street, so what more is expected to happen to it?

My other concern is what happened to my Twin Brooks Neighborhood Association and designation. They put in the neighborhood boundary markers, which look great, but the neighborhood association is no longer listed on the city's map of associations. What happened? We're not Child's Park and have a different set of concerns and priorities than that group.

So, in sum, my major concerns are the definition of FMS within 175 feet of my house and what that allows the city to do to that street. And what happened to TBNA, and how to get us back on the map.

Please get back to me.

Sincerely,

Lane Driscoll  
2225 Quincy Street S  
St. Petersburg

## Ann O. Vickstrom

---

**From:** Karen Lorenz <lorenz.karen2@gmail.com>  
**Sent:** Thursday, January 26, 2023 9:45 AM  
**To:** Ann O. Vickstrom  
**Subject:** Rezoning ONE St Pete

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good morning,

I am reaching out to you to share my concerns and objections to the proposed rezoning of properties from NT to NTM-1. Although I am strongly **opposed to it anywhere in the Old Northeast**, I will focus on my concerns about increased accessory dwelling units, duplexes, triplexes and fourplexes in Greater Woodlawn i.e. along 22<sup>nd</sup> Ave N. I will call them out in bullet points for clarity and focus.

1. Already existent issues with water and particularly sewer along 22<sup>nd</sup> Ave N between MLK and 16<sup>th</sup> St. N. The cast iron sewer/drainage pipes under the blacktop of 22<sup>nd</sup> Ave N have had multiple issues over the last few years. There have been frequent closures of the center lane on 22<sup>nd</sup> Ave N to try to remediate the problem and my concern is that with the addition of this many units/bathrooms it will exacerbate the issue.
2. Sanitation Sewers for residents, also cast iron, are located in the alley.
3. Alleys are mostly either dirt or brick and contain many indentions. This will be an issue if alley traffic increases due to more residences.
4. **PARKING!** As there are few driveways off of 22<sup>nd</sup> Ave N, most residents park in the alley behind their home. There are few places currently, and there are frequent issues with guests of a neighbor parking in another homeowners parking area or driveway. Increased population density will bring this issue to a tipping point.
5. Large trashcans are located in the alley. If population increases, more will be required and there is barely enough space for the ones that already exist.
6. Increased traffic will be problematic, increasing the already aggravating noise and vibrations along major throughfares.
7. Construction vehicles would take over existing parking areas during destruction/construction and interfere with sanitation pick up.
8. Greater Woodlawn is fast approaching 100 years since incorporation. Changes like this would damage the historic feel and appeal of the area.
9. Units such as this were incorporated into Crescent Heights and ruined the atmosphere of the neighborhood.

Again, I am only calling out the issues that make me **blatantly opposed** to the prospect but I am sure this opposition is felt by most of the neighboring areas as well.

I heartily request the Zoning changes be **ADAMENTLY** denied for these areas of Old Northeast.

Regards  
Karen Lorenz

## Ann O. Vickstrom

---

**From:** Norma Bouillion <nbouillion@yahoo.com>  
**Sent:** Friday, January 27, 2023 4:09 PM  
**To:** Ann O. Vickstrom  
**Subject:** Rezoning

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am a resident of St Petersburg that will greatly be affected by the cities plan to rezone for more affordable housing. I'm writing to express the discontent my husband and I have towards this plan. We purchased a home off 22nd just north of the alley where 4 home sites/condos can be built right outside our back yard. We would not have paid the premium price for our house 1 1/2 year ago had we known this would happen. How can the city add more traffic to 22nd? It's busy now. When the mayor was running his campaign was "more affordable housing" where the ball field was. now it looks like those are going to be for first responders teachers, etc. with new businesses, restaurants etc.... Raising property values creating a beautiful downtown area but sticking the affordable housing within our residential areas. What is this going to do to our property values? does the city honestly think we will pay these high taxes up against these kind of properties? this plan has definitely made for some very very unhappy residents.

Sincerely,  
Norma Bouillion.



## Ann O. Vickstrom

---

**From:** Mike Battigelli <mikebattigelli@gmail.com>  
**Sent:** Saturday, January 28, 2023 12:29 PM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am NOT in favor of proposed NTM-1MAP Amendment.. Put in the Mayor's Neighborhood.

## Ann O. Vickstrom

---

**From:** Corey Vongsalay <outlook\_ED449F0474B0C053@outlook.com>  
**Sent:** Sunday, January 29, 2023 8:02 PM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am NOT in favor of the proposed NTM-1 Map Amendment.

Sincerely,  
Corey Vongsalay

Sent from [Mail](#) for Windows

## Ann O. Vickstrom

---

**From:** Susan Fulmer <kellyfulmer1221@gmail.com>  
**Sent:** Monday, January 30, 2023 10:00 AM  
**To:** Ann O. Vickstrom  
**Subject:** ZM-15: NTM-1 Rezoning Application

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Ms. Vickstrom,

I live on 9 Ave N. In Old Northeast ( soon the adjective "old" will no longer be needed with all the old homes being torn down).

I am totally against the proposal to changing the zoning in my neighborhood. My question to you, when will the nonstop development end? And building condos, townhomes, duplexes, tri-plex etc built in Old Northeast or other surroundings neighborhoods will not be "affordable" housing. The developers will make their money and move on, it's about \$\$\$ not about improving the neighborhoods.

Where do you live? Will this affect your neighborhood?

Please do the right thing by the residence who actually live in these neighborhoods.  
Sent from my iPad

## Ann O. Vickstrom

---

**From:** Mainstream America <mainstreamamerica@tampabay.rr.com>  
**Sent:** Wednesday, February 01, 2023 9:57 AM  
**To:** Ann O. Vickstrom  
**Subject:** DRC Agenda Item #1 - LDR2022-04

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Ms. Vickstrom,

I strongly object to the inclusion of 30<sup>th</sup> Avenue N in the new NTMR-1 zoning category. This road is a narrow two lane road bordered by single story, single family residential for the most part. While there is some commercial zoning at nodes, (4<sup>th</sup> St, MLK St, 16<sup>th</sup> St, etc.), the overwhelming use and character is residential. The areas on 30<sup>th</sup> slated for rezoning have, for the most part, unimproved dirt alleys behind them and I see no provision for requiring them to be paved. Dirt alleys sustaining the amount of traffic generated by this increase in density would require virtually monthly maintenance by the City. If the developers aren't required to pave the alleys, then the residents behind the fourplexes will be assessed to do so – something that hardly seems fair since they'll take the property values hit for the increased density.

And there WILL be increased traffic. The condition of the road does not lend itself to increased density despite the “rehab” a few years back. Stormwater collects along either side of the roadway and stays for days, fouling the underutilized bike lanes and making them impassible. The roadway continues to subside, leaving gaping holes at times, due to unrepaired leaking city pipes underneath the roadbed, another issue not fixed during the rehab. There are no contiguous sidewalks along the south side of 30<sup>th</sup> between MLK and 16<sup>th</sup> Streets, again, something not addressed in the rehab. Furthermore, there are no buses on 30<sup>th</sup> Avenue west of MLK. Where are the walkers and bike riders to go and how will they get there - safely?

In reviewing the plan and the map, I note that the rezoning is not slated for 1<sup>st</sup> Street N/NE and I truly do not understand why not. This street actually has sidewalks AND bus service. I think a more comprehensive look at the map and the proposed areas for rezoning with an emphasis on what actually exists in the areas to be rezoned should be undertaken before this ordinance is brought to City Council for readings. The 30<sup>th</sup> Avenue road, at least from MLK west, should be removed from consideration.

Regards,

Cathy Wilson  
1471 29<sup>th</sup> Avenue N  
St Petersburg, FL 33704  
Phone: (727) 455-9546

## Ann O. Vickstrom

---

**From:** Susan Fulmer <kellyfulmer1221@gmail.com>  
**Sent:** Wednesday, February 01, 2023 12:14 PM  
**To:** Ann O. Vickstrom  
**Subject:** Re: ZM-15: NTM-1 Rezoning Application

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Afternoon-

I live at 120 9th Ave N. The Rezoning may not affect me at this time, but I have no doubt it is just a matter of time if it's allowed in local neighborhoods.

And the term used in the Rezoning of "Future" Major Roads/ Streets is concerning.

There is plenty of development downtown, plenty of apartments and condos, that is where multi units need to remain.

Susan Fulmer

Sent from my iPad

> On Jan 30, 2023, at 11:13 AM, Ann O. Vickstrom <Ann.Vickstrom@stpete.org> wrote:

>

> Ms. Fulmer,

>

> Thank you for your comments. Your comments are noted and will be attached to the staff report that is sent to City Council.

>

> I would like to clarify that the proposal is not along 9th Avenue in the Historic Old Northeast neighborhood. However, there are several properties on 9th Avenue between 4th Street and Dr. ML King Jr St. Most of these properties currently have multi-family units. Can you let me know your address so I can determine if there are any properties around your property proposed for this rezoning?

>

> Respectfully,

>

> Ann Vickstrom, AICP, RLA#0001122

> Planner II, Urban Planning and Historic Preservation City of St.

> Petersburg

> (727)892-5807

> Ann.Vickstrom@stpete.org

>

> Please note all emails are subject to public records law.

>

>

>

> -----Original Message-----

> From: Susan Fulmer <kellyfulmer1221@gmail.com>

> Sent: Monday, January 30, 2023 10:00 AM

> To: Ann O. Vickstrom <Ann.Vickstrom@stpete.org>  
> Subject: ZM-15: NTM-1 Rezoning Application  
>  
> CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.  
>  
>  
> Dear Ms. Vickstrom,  
>  
> I live on 9 Ave N. In Old Northeast ( soon the adjective "old" will no longer be needed with all the old homes being torn down).  
>  
> I am totally against the proposal to changing the zoning in my neighborhood. My question to you, when will the nonstop development end? And building condos, townhomes, duplexes, tri-plex etc built in Old Northeast or other surroundings neighborhoods will not be "affordable" housing. The developers will make their money and move on, it's about \$\$\$ not about improving the neighborhoods.  
>  
> Where do you live? Will this affect your neighborhood?  
>  
> Please do the right thing by the residence who actually live in these neighborhoods.  
> Sent from my iPad  
>  
> <<http://www.stpete.org/vision>>  
>

## **Rezone Neighborhood Traditional (NT) to Neighborhood Traditional Mixed Residential (NTM-1)**

### **Facts:**

1. Redesignation of streets to major changes the character of the neighborhood. Future major corridors include single family housing-dominated residential streets such as 30<sup>th</sup> Ave. N. that currently are appropriately designated as minor arterial roads.
2. Alley traffic impacts are not addressed. Adjacent parcels will be negatively impacted by an increase of up to 150% in alley traffic. If each unit has two vehicles, and every lot is redeveloped as multi-family, the increase would be from two to eight vehicles per parcel, or as many as 40 vehicles per block.
3. Additional traffic on public alleys will necessitate improvements such as resurfacing, signage and speed controls. Maintenance costs will increase. Safety will be degraded (limited sightlines, 10 mph speed limit). The net impact is to reduce adjacent parcel value while degrading neighborhood character.
4. The maximum value of parcels currently zoned for single family housing will increase greatly if up to four units may be built on the same lot. Developers have and will buy entire rows of adjacent property on NTM-1 zoned streets.
5. The value of adjacent parcels, such as our new home on 29<sup>th</sup> Ave. N, will decrease because the character of the neighborhood is not respected.
6. Parcels in affluent neighborhoods such as Woodlawn and Allendale Terrace on major corridors are not included in rezoning due to lack of alleys. Other affluent neighborhoods are not included in rezoning (e.g. Old NE, others).
7. Existing multi-family housing in NT is about 100 years old and is high maintenance and functionally obsolete, yet the ability to replace them in-kind is currently not allowed. Replacement in-kind would not change neighborhood character, and property values would be preserved.
8. Distribution of affordable housing within affluent neighborhoods preserves neighborhood character and is comprehensively better for everyone than concentration along major streets.
9. Tenants prefer to rent directly from individual owners. High density affordable housing will not be directly managed by small business owners, rather by corporations lacking the human element that contributes to neighborhood character and quality.

### **Recommendations:**

1. Do not cause harm to existing single family-zoned residential streets by changing their character, reconsider plans for 30<sup>th</sup> Ave. N. use as a major corridor, to avoid degrading property values of adjacent parcels and their adjoining neighborhoods.
2. Limit density in NTM-1 to one multi-family building per block to minimize harmful impact to neighbors. The rezoning could be applied city-wide if this were the rule.
3. Encourage redevelopment of existing Middle LDR dwellings with appropriate density housing (up to four units) to reduce the concentration of increased density housing and preserve the small business owner-tenant ideal.

4. Alternatively, avoid discrimination by rezoning to NT all major corridors without exception, or none.

John Deas & Mary Romanik  
1445-29<sup>th</sup> Avenue N.  
St. Petersburg



**LDR 2023-01: NTM-1 LDR Text Amendment**

Presented to Development Review Commission

On February 1, 2023



**Staff Report to the St. Petersburg Development Review Commission (DRC)**

Prepared by the Planning & Development Services Department  
Urban Planning and Historic Preservation Division

For Public Hearing on Wednesday, February 1, 2023  
at 1:00 p.m. in the City Council Chambers, City Hall  
175 Fifth Street North, St. Petersburg, Florida.

**City File: LDR 2023-01**  
**Missing Middle Housing Text Amendments**

This is a City-initiated application requesting that the Development Review Commission (“DRC”), in its capacity as the Land Development Regulation Commission (“LDRC”), make a finding of consistency with the Comprehensive Plan and recommend to City Council APPROVAL the following text amendment to the City Code, Chapter 16, Land Development Regulations (“LDRs”).

**APPLICANT INFORMATION**

**APPLICANT:** City of St. Petersburg  
175 5<sup>th</sup> Street North  
St. Petersburg, Florida 33712

**CONTACT:** Ann Vickstrom, Planner  
Urban Planning and Historic Preservation Division  
Planning and Development Services Department  
Ann.Vickstrom@stpete.org  
(727) 892-5807

Derek Kilborn, Manager  
Urban Planning and Historic Preservation Division  
Planning and Development Services Department  
Derek.Kilborn@stpete.org  
(727) 893-7872

**BACKGROUND**

This application is a text amendment to the LDRs. A companion application rezoning qualified parcels to the NTM-1 zoning designation is being separately processed through the City’s Community Planning and Preservation Commission (“CPPC”) as an amendment to the Official Zoning Map. The zoning map amendment is identified as Application No. ZM-15 and is scheduled for public hearing before the CPPC on Tuesday, February 14, 2023, starting at 2:00 PM. Both the rezoning application and this text amendment application will be presented to the City Council on March 2 and March 23, 2023.

In November 2019, as part of a four-part initiative to amend City land development regulations to address affordable housing, City Council approved the Neighborhood Traditional Mixed Residential (NTM-1) district regulations allowing up to four (4) units on a typical sized single-family lot (calculated at 30 units per acre). Following that approval, due to the COVID pandemic and subsequent changes to operational procedures in 2020, the proposed NTM-1 zoning map amendment was placed on hold. After completion of the StPete2050 Plan in May 2021, staff met with City Council on five occasions from August 2021 through August 2022 to get direction on next steps. The amendment addressing Accessory Dwelling Units was approved in July 2022 and these companion amendments address the application of Missing Middle housing on qualifying properties.

## REQUEST

During the evaluation of the NTM rezoning initiative, staff identified an opportunity to allow Missing Middle housing in four additional zoning districts and determined that several changes to the previously approved NTM regulations were needed to provide better clarify the district standards. The amendments can be summarized as follows:

- Text amendments to allow a Missing Middle density bonus on qualified parcels in the corridor zoning categories and the Neighborhood Suburban multi-family district, following all dimensional and design requirements of the Neighborhood Traditional Mixed Residential (NTM) district.
- NTM-1 district amendments clarifying applicability requirements, setbacks, landscaping, parking and providing a definition.
- Correcting omissions from the ADU ordinance adopted in July 2022 (LDR 2022-01)

The amendments are more particularly described below in numerical order, and a Strike-through/Underline version of the code amendments is attached.

1. Amend the definition of Multi-family and add NTM in the Traditional Tier parking category on the Use Permissions and Parking Matrix:

*Section 16.10.020.1. - Matrix: Use permissions and parking requirements matrix and zoning matrix.* This change expands the multi-family definition to include a “lot” as a multi-family use of two (2) or more units. The current definition only refers to a “structure” and thereby limits the option of having multiple single-family buildings on one property, for example, a cottage court. This change allows the type of design flexibility contemplated within the NTM-1 zoning category. The 2019 amendment did not specify the parking tier for the NTM district and inclusion in the *Traditional Tier* identifies parking requirements for NTM.

2. Amend the Maximum development potential tables in the NT-3, NS-1 and NS-2 Districts

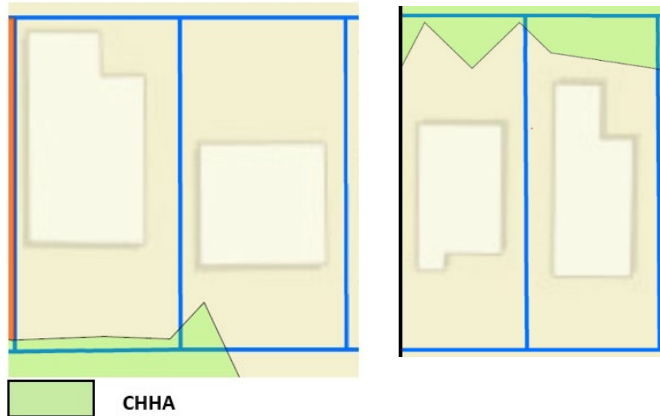
*Sections 16.20.010.5 and 16.20.020.6 Maximum development potential.* This change to the tables was erroneously excluded in the July 2022 application (LDR 2022-1), when the ADUs were expanded into the NT-3, NS-1, and NS-2 districts.

3. Amend the NTM applicability standards

*Section 16.20.015.2. – Applicability.* This proposed change eliminates the requirement for a property to be located within a traditional neighborhood, clarifies when a parcel is partially located in the Coastal High Hazard Area (CHHA), and relocates standards related to the National Register and Local Historic Districts to the design standard section.

During research for this application and its companion zoning map amendment, it was determined that clarification is needed for parcels that are partially located within the CHHA. This text amendment is proposing to allow parcels to qualify provided at least 75% or more of the property is located outside the CHHA, and there is no increase in density allowed on the parcel for any portion of the parcel located in the CHHA. Two examples are shown in Figure 1 below:

Figure 1. Properties partially in CHHA: more than 75% of land is outside CHHA



4. Amend the minimum lot size standards for the NTM-1 District.

*Table 16.20.015.4.a: Minimum Lot Standards and Lot Coverage.* This proposed change is to clarify the minimum lot size. The lot size of 1,452 square feet (SF) per unit was based on 30 units/acre. However, this lot size is not consistent with any current neighborhood pattern. A minimum lot size for of 2,000 square feet and width of 20 feet will provide flexibility to design a narrow single-family home and maintain consistency with existing patterns of development.

5. Amend the NTM-1 Minimum Building Setback table to include setbacks for National or Local Historic District properties that are consistent with the existing zoning standards and patterns of development.

This proposed text amendment will address concerns raised by residents in historic districts, requiring that setbacks remain consistent with the existing NT zoning categories. Modifications can be made through section 16.20.015.6, if consistent with the established neighborhood pattern. Although this text amendment applies to both the National Register and Local Historic Districts, new construction within Local Historic Districts is subject to public hearing review and approval of a Certificate of Appropriateness (“COA”).

6. Clarify entrance requirement for bungalow courts.

*Table 16.20.015.7: Entrances.* This amendment clarifies that the entrance to the unit that abuts the primary street must face the street.

7. Amend the *Building and Site Design* standards.

Section 16.20.015.8. – *Building and Site Design* is proposed to be amended as follows:

- For properties located with National Register and Local Historic Districts, design standards are relocated from the Applicability section, requiring that the addition of dwelling units be within the existing principal structure or as part of an addition or within an accessory building, when designed subordinate to the principal structure.
- Clarifies landscape requirements for up to four units on a parcel, allowing the use of the one- and two-unit residential standards instead of commercial parking lot standards.

- Provide clarification for non-conforming front and street side driveways and parking. This amendment specifically allows existing front or street side driveways and parking to remain when additional units are added or if existing buildings are retained on site, provided that all parking and driveways meet the dimensional design standards for parking and adds a requirement that any front parking be separated from the sidewalk and screened with a fence and a minimum 3-foot landscape buffer, to prevent cars from blocking the sidewalk and provide a visual buffer.
8. Allow a density bonus in the Neighborhood Suburban Multi-family NSM-1 District, Corridor Residential Traditional CRT-1 District, Corridor Commercial Traditional CCT-1 District, and Corridor Commercial Traditional CCS-1 District to qualified parcels when Missing Middle housing is provided.

This text amendment allows for a density bonus when Missing Middle housing is proposed within existing mixed-use corridors and the neighborhood suburban multi-family district subject to the applicability, design and parking requirements of the NTM category. A property owner may develop under the existing zoning category or opt to use the Missing Middle housing density bonus of up to 30 units per acre provided the development meets all NTM-1 regulations. This density bonus is not allowed in addition to the Workforce Housing density bonus. Staff estimates that this change will apply to approximately 1,700 parcels citywide on the major streets.

9. Provide a definition for Missing Middle Housing

*Section 16.90.020.3. – Definitions.* Add a definition to the LDRs, consistent with existing definitions in the City’s Comprehensive Plan and Countywide Rules.

**CONSISTENCY TO THE COMPREHENSIVE PLAN**

The following objectives and policies from the City’s Comprehensive Plan are applicable to the **Compliance of the proposed use with the goals, objectives, policies, and guidelines of the Comprehensive Plan.**

LU3.6 Land use planning decisions shall weigh heavily on the established character of predominately developed areas where changes of use or intensity of development are contemplated.

*The NTM-1 development standards are intended to maintain neighborhood compatibility in building placement, scale, and design. A number of the text amendments proposed here will strengthen these protections first adopted in 2019 by adding clarifications and eliminating potential loopholes. When applied to the Official Zoning Map, the qualified properties are located along Future Major Streets. These streets have more intensive traffic than the internal neighborhood streets. In addition, many of these streets also include the City’s transit routes, which support higher densities by providing multi-modal options and other transportation alternatives.*

LU3.11 More dense residential uses (more than 7.5 units per acre) may be located along (1) passenger rail lines and designated major streets or (2) in close proximity to activity centers where compatible.

*The NSM-1, CRT-1, CCT-1 and CCS-1 districts are located along Future Major Streets and transit lines. The option to develop under the NTM-1 regulations is consistent to this policy.*

- LU3.15 The Land Use Plan shall provide housing opportunity for a variety of households of various age, sex, race and income by providing a diversity of zoning categories with a range of densities and lot requirements.

*The proposed text amendments provide the allowance of missing middle housing which refers to houses that are on the building spectrum between single-family and multi-unit buildings as an optional development in NSM-1, CRT-1, CCT-1 and CCS-1 districts. Buildings such as duplexes and townhouses contribute to the diversity of housing options both in form and affordability. Developing Missing Middle housing increases the housing stock while catering to a variety of demographics including multigenerational households that are looking for smaller homes in walkable neighborhoods.*

- LU4 The following future land use needs are identified by this Future Land Use Element: 1. Residential – the City shall provide opportunities for additional residential development where appropriate.

*The proposed text amendments strengthen an existing ordinance allowing a diversity of housing typologies that are compatible with existing residential neighborhoods in the traditional context.*

- LU22.1 The City shall continue to pursue strategies which reduce GHG emissions and vehicle miles traveled.

*Providing NTM-1 zoning along transit routes, corridors and Future Major Streets will potentially reduce Greenhouse Gas Emissions (“GHG”) emissions as it provides alternative transportation to the residents of the neighborhood and allows for walkability along major corridors to retail and services rather than driving.*

- LU23.3 The City’s LDRs shall continue to support greater development intensity within the Corridor and Center zoning districts, particularly where located along fixed transit lines and around transit stops and stations.

*The proposed map and text amendments are proposed along the Future Major Streets and corridors where transit lines and stops are provided.*

- LU23.3 The City’s LDRs shall continue to support greater development intensity within the Corridor and Center zoning districts, particularly where located along fixed transit lines and around transit stops and stations.

*The proposed map and text amendments are proposed along the Future Major Streets and corridors where transit lines and stops are provided.*

- CM10B The City shall direct population concentrations away from known or predicted coastal high hazard areas consistent with the goals, objectives, and policies of the Future Land Use Element.

*These text amendments propose to include parcels where 75% or more of the property is located outside of the CHHA. This is proposed where the CHHA has a minimal effect on the property. This allowance clarifies existing language regarding partially included parcels. Given that no increase in density will be allowed for that portion of the property*

*in the CHHA, the text amendment is not considered a concentration of population within the CHHA.*

- T1.6 The City shall support high-density mixed-use developments and redevelopments in and adjacent to Activity Centers, redevelopment areas and locations that are supported by mass transit to reduce the number and length of automobile trips and encourage transit usage, bicycling and walking.

*The proposed amendment includes areas adjacent to Activity Centers, in redevelopment areas and supported by mass transit to reduce the number and length of automobile trips and encourage transit usage, bicycling, and walking as provided in the Complete Streets program.*

## **PUBLIC OUTREACH**

Starting in August 2022, the Planning and Development Services Department staff responded to individual inquiries and neighborhood association invitations and hosted five (5) workshops pertaining specifically to this LDR 2023-01 application including:

- 11/14/2022 ZM-15/LDR 2023-01 NTM-1 Map and Text Amendments Workshop @ Childs Park
- 11/15/2022 ZM-15/LDR 2023-01 NTM-1 Map and Text Amendments Workshop @ Roberts Recreation Center
- 11/16/2022 ZM-15/LDR 2023-01 NTM-1 Map and Text Amendments Virtual Workshop
- 01/11/2023 Crescent Lake Neighborhood Association Meeting
- 01/12/2023 **DRC LDR 2023-01 Text Amendment Workshop**

Public feedback included a diversity of subjects, comments, and concerns that are reflected in the evolution of drafts previously presented and the final set of recommendations proposed herein. A public comment report is attached. Specifically, discussion focused on the front loaded driveways and parking spaces. Correspondence provided regarding the text amendments is attached.

## **PUBLIC HEARING PROCESS**

The LDR text amendment requires one public hearing before the Development Review Commission (DRC) text amendment require two City Council public hearings.

## **RECOMMENDATIONS**

Staff recommends that the Development Review Commission, in its capacity as the Land Development Regulation Commission, make a finding of consistency with the City's Comprehensive Plan and recommend to City Council APPROVAL of the City Code, Chapter 16 LDR 2023-01 Missing Middle text amendments as described herein.

**HOUSING AFFORDABILITY IMPACT STATEMENT**

**City of St. Petersburg  
Housing Affordability Impact Statement**

Each year, the City of St. Petersburg receives approximately \$2 million in State Housing Initiative Partnership (SHIP) funds for its affordable housing programs. To receive these funds, the City is required to maintain an ongoing process for review of local policies, ordinances, resolutions, and plan provisions that *increase the cost of housing construction, or of housing redevelopment*, and to establish a tracking system to estimate the cumulative cost per housing unit from these actions for the period July 1– June 30 annually. This form should be attached to all policies, ordinances, resolutions, and plan provisions which increase housing costs, and a copy of the completed form should be provided to the City’s Housing and Community Development Department.

**I. Initiating Department:** Planning & Development Services Development

**II. Policy, Procedure, Regulation, or Comprehensive Plan Amendment Under Consideration for adoption by Ordinance or Resolution:**

See attached amendment to Chapter 16, City Code of Ordinances (City File LDR 2022-01).

**III. Impact Analysis:**

A. Will the proposed policy, procedure, regulation, or plan amendment, (being adopted by ordinance or resolution) increase the cost of housing development? (i.e. more landscaping, larger lot sizes, increase fees, require more infrastructure costs up front, etc.)

No  (No further explanation required.)

Yes  Explanation:

If Yes, the **per unit cost increase** associated with this proposed policy change is estimated to be: \$ \_\_\_\_\_.

B. Will the proposed policy, procedure, regulation, plan amendment, etc. increase the time needed for housing development approvals?

No  (No further explanation required)

Yes  Explanation:

**IV: Certification**

X: It is important that new local laws which could counteract or negate local, state and federal reforms and incentives created for the housing construction industry receive due consideration. If the adoption of the proposed regulation is imperative to protect the public health, safety and welfare, and therefore its public purpose outweighs the need to continue the community’s ability to provide affordable housing, please explain below:

The proposed regulation, policy, procedure, or comprehensive plan amendment will **not** result in an increase to the cost of housing development or redevelopment in the City of St. Petersburg and no further action is required. (Please attach this Impact Statement to City Council Material and provide a copy to Housing and Community Development department.)

*/s/ Elizabeth Abernethy*

Director, Planning & Development Services (signature)

01-25-2023

Date

Copies to: City Clerk; Joshua A. Johnson, Director, Housing and Community Development



**Proposed Text Amendments**

**LDR 2023-01 Missing Middle Text Amendments  
Proposed LDR Amendments**

Section 16.10.020.1. of the St. Petersburg City Code, excerpted in pertinent part, is hereby amended to read as follows:

**16.10.020.1. - Matrix: Use permissions and parking requirements matrix and zoning matrix.**

Use	Minimum Parking Spaces, Traditional Tier (NT, NTM, CRT, CCT-1, IT)	Definitions
<b>Dwelling, Multifamily</b>	More than 750 square feet: 0.75 per unit up to 2 bedrooms, plus 0.5 for each additional bedroom; Equal to or less than 750 square feet: 0.50 per unit; Loading area required for more than 5 units	A building <b>or lot</b> designed for or occupied by two or more families (on the basis of monthly, or longer occupancies, or ownership of individual units) with separate cooking, bathroom and sleeping facilities for each unit. Motels, hotels, and other transient accommodation uses are not multiple-family dwellings. Accessory uses include clubhouses, recreational and laundry facilities

**16.20.010. NEIGHBORHOOD TRADITIONAL SINGLE-FAMILY DISTRICTS (NT)**

**16.20.010.5. Maximum development potential.**

Development potential is different within each district in order to respect the character of the neighborhoods. Achieving maximum development potential will depend upon market forces, such as minimum desirable unit size, and development standards, such as minimum lot size, parking requirements, height restrictions, floor area ratios, maximum building and impervious surface ratios, and building setbacks.

To maintain community character and provide for desirable redevelopment and infill housing, homes shall be built using FARs as set forth herein. Various design standards may be used to increase the FAR and maintain the compatibility of new and modified homes with the existing neighborhood character. Therefore a maximum FAR is established and FAR bonuses may be permitted if the home incorporates design elements as set forth herein which are intended to be beneficial to the character of the neighborhood and reduce the appearance of mass and bulk from the public view.

## Minimum Lot Size, Maximum Density and Maximum Intensity

		NT-1	NT-2	NT-3	NT-4
Minimum Lot Width	Residential	45 ft.	50 ft.	60 ft.	45 ft.
	Nonresidential	180 ft.	200 ft.	240 ft.	180 ft.
Minimum Lot Area (square feet)	Residential	4,500	5,800	7,620	5,800
	Nonresidential	22,860	25,400	30,480	22,860
Maximum Residential Density (units per acre)		15 (1 principal unit and 1 accessory unit per lot) <sup>(1)</sup>	15 (1 principal unit and 1 accessory unit per lot) <sup>(1)</sup>	7 (1 principal unit; and 1 accessory unit per lot) <sup>(1)</sup> <del>not permitted</del>	15 (1 principal unit and 1 accessory unit per lot) <sup>(1)</sup>
Maximum Residential Intensity (floor area ratio) <sup>(2)(3)</sup>		0.50	0.40	0.40	0.50
Maximum Nonresidential Intensity (floor area ratio)		0.50	0.50	0.40	0.85
Maximum Residential Building Coverage (includes all enclosed structures) except where the primary structure is one story then a 0.60 building coverage is allowed		0.55	0.55	0.55	0.55
Maximum Impervious Surface (site area ratio)	Residential	0.65	0.65	0.65	0.65
	Nonresidential	0.55	0.55	0.55	0.55

(1) Refer to use specific development standards for regulations regarding development of accessory dwelling and accessory living space.

**16.20.015. NEIGHBORHOOD TRADITIONAL MIXED-RESIDENTIAL DISTRICT (NTM-1)**

**16.20.015.2. Applicability.**

Uses in this district shall be allowed as provided in the Matrix: Use Permissions and Parking Requirements.

A. Applicable to locations that transition from a mixed-use corridor, center or Future Major Street to a single-family neighborhood. The most effective application of this district is in a linear configuration when located within 175-feet of the centerline of a designated *Future Major Street* or *High Frequency Transit Route* with service head-way times equal to, or less than, 35-minutes.

1. Qualified properties shall be adjacent to a public alley.
2. Applicable to traditional neighborhoods, where the subject property:
  - a. Retains direct connectivity to one or more adjoining *Future Major Streets* or *High-Frequency Transit Routes*; and
  - b. A minimum of 75% of the property is outside of the designated Coastal High Hazard Area ("CHHA"), and in no case shall the density in the CHHA be increased.

~~3. Where listed in the St. Petersburg Register of Historic Places as an individual local landmark or contributing resource to a local historic district, or where listed in the National Register of Historic Places as an individual listing or contributing resource to a historic district, new dwelling units above the existing number of dwelling units shall only be allowed when adaptively established within the existing principal structure additions and accessory buildings may include new dwelling units when designed subordinate to the principal structure and in accordance with the applicable review procedures.~~

**TABLE 16.20.015.4.a: Minimum Lot Standards and Lot Coverage**

Lot Standards	
Lot Area, Minimum: Residential	1,452 2,000 square feet per unit
Lot Area, Minimum: Non-Residential	22,860 square feet
Lot Width, Minimum: Residential	20 feet
Lot Width, Minimum: Non-Residential	150 feet
Lot Coverage	
Impervious Surface, Maximum: Residential	0.75 or 75%
Impervious Surface, Maximum: Non-Residential	0.65 or 65%
Building Coverage, Maximum <sup>1</sup> : Residential	0.60 or 60%
<sup>1</sup> Includes all enclosed structures	

**TABLE 16.20.015.5.b: Minimum Building Setbacks**

Building Setbacks <sup>1,2,3</sup>	
Front: Steps Extending from Porch or Stoop <sup>4</sup>	8-feet or M
Front: Porch or Stoop <sup>4</sup>	12-feet or M
Front: Building <sup>4</sup>	18-feet or M
Side, Interior <sup>4</sup>	3-feet or M
Side, Street <sup>4</sup>	8-feet or M
Rear, Alley	22-feet, including width of alley
Special Exception	
All yards	25-feet
<sup>1</sup> M (minor encroachment): Minor encroachments into normally prescribed setbacks may be allowed in order to accommodate an addition to align with the side of the existing structure, provided:	

(a) The total floor area of the encroaching portion of an addition shall not exceed 50 square feet;
(b) No portion of the encroachment shall exceed 24 feet in height.
<sup>2</sup> Refer to technical standards regarding measurement of building setbacks and setback encroachments.
<sup>3</sup> The larger of the minimum building separation distances required by the Florida Building Code or the Life Safety Code or the minimum building setback established for the interior side yard setback shall apply.
<sup>4</sup> When located within a National or Local Historic District – Front: Building 25-feet; Front: Porch or Stoop 18-feet; Side: street 12-feet; Side: Interior 5-feet.

**TABLE 16.20.015.7: Entrances**

Accessory Dwelling Unit ("ADU")	Per ADU standards
Detached House	1 entrance facing the primary street
Duplex	1 entrance minimum, 2 entrances maximum, facing the primary street; on corner lots, each unit entrance shall face a different street, except where entrances are provided from within an interior vestibule or hallway.
Triplex and Fourplex	1 entrance minimum, 2 entrances maximum, facing the primary street; on corner lots, each unit entrance shall face a different street, except where entrances are provided from within an interior vestibule or hallway.
Bungalow Court	Each main entrance shall face the shared court, <u>except</u> cottages abutting the primary street shall have their main entrance facing the primary street.
<sup>1</sup> Pedestrian connections shall link each exterior entrance to the public rights-of-way, private open space, and parking areas.	

**16.20.015.8. Building and site design.**

The following design criteria allow the property owner and design professional to choose their preferred architectural style, building form, scale and massing, while creating a framework for good urban design practices.

*Site layout and orientation.* The City is committed to creating and preserving a network of linkages for pedestrians.

Local and National Historic Districts

1. Where listed in the St. Petersburg Register of Historic Places as an individual local landmark or contributing resource to a local historic district, or where listed in the National Register of Historic Places as an individual listing or contributing resource to a historic district, the addition of dwelling units shall only be allowed when adaptively established within the existing principal structure or within an addition or accessory building when designed subordinate to the principal structure.
2. *Building layout and orientation.*
  1. For non-residential uses, all service areas and loading docks shall be located behind the front façade line of the principal structure.
  2. All mechanical equipment and utility functions (e.g. electrical conduits, meters and HVAC equipment) shall be located behind the front façade line of the principal structure. Mechanical equipment that is visible from the primary street shall be screened with a material that is compatible or consistent with the architecture of the principal structure.
  3. Accessory structures (including sheds) shall be located behind the front façade line of the principal structure.

**Landscaping.** For up to 4-units on a property, landscaping shall meet the requirements of Section 16.40.060.2.1.1 Development and redevelopment of new one- and two-unit residential properties.

*Vehicle connections and parking.*

1. All parking shall be accessed from a public alley.
2. Garage doors shall face the alley.
3. All parking spaces shall be located behind the plane of the front building face.
4. Notwithstanding the foregoing, non-conforming front or side street driveways and parking may remain when additional units are added to a building or if existing buildings are retained on site, provided that all parking and driveways meet the design standards of 16.40.090, parking spaces be separated from the sidewalk and screened with a fence and a minimum 3-foot landscape buffer, and any additional parking be accessed from a public alley. Further, where an existing driveway is providing access to a single-family garage, the driveway may remain regardless of the number of units added to the site, provided any parking for the additional units is accessed from the alley.

**16.20.020. NEIGHBORHOOD SUBURBAN SINGLE-FAMILY DISTRICTS (NS)**

**16.20.020.6. Maximum development potential.**

Development potential is slightly different within each district to respect the character of the neighborhoods. Achieving maximum development potential will depend upon market forces, such as minimum desirable unit size, and development standards, such as minimum lot size, parking requirements, height restrictions, floor area ratios, maximum building and impervious surface ratios, and building setbacks.

To maintain community character and provide for desirable redevelopment and infill housing, homes shall be built using FARs as set forth herein. Various design standards may be used to increase the FAR and maintain the compatibility of new and modified homes with the existing neighborhood character. Therefore a maximum FAR is established and FAR bonuses may be permitted if the home incorporates design elements as set forth herein which are intended to be beneficial to the character of the neighborhood and reduce the appearance of mass and bulk from the public view.

Minimum Lot Size, Maximum Density and Maximum Intensity

		<b>NS-1</b>	<b>NS-2</b>	<b>NS-E</b>
Minimum lot width	Residential	75 ft.	100 ft.	200 ft.
	Nonresidential	150 ft.	200 ft.	200 ft.
Minimum lot area	Residential	5,800 sq. ft.	8,700 sq. ft.	1.0 acre
	Nonresidential	1.0 acre	1.0 acre	2.0 acres
Maximum residential density (units per acre)		7.5 1-principal unit and 1 accessory unit per lot (see note 1) not permitted	5 1-principal unit and 1 accessory unit per lot (see note 1) not permitted	2 1 principal unit and 1 accessory unit per lot (see note 1)
Maximum Residential Intensity (floor area ratio) <sup>(2)</sup>		N/A	N/A	N/A
Maximum nonresidential intensity (floor area ratio)		0.35	0.30	0.20
Maximum Residential Building Coverage (includes all enclosed structures) except where the primary structure is one story then a 0.60 building coverage is allowed		0.55	0.55	0.55
Maximum impervious surface (site area ratio)		0.60	0.60	0.40

(1) Refer to use specific development standards for regulations regarding development of accessory dwelling and accessory living space.

**16.20.030. NEIGHBORHOOD SUBURBAN MULTIFAMILY DISTRICTS (NSM)**

**16.20.030.4. Introduction to NSM districts.**

The NSM districts are the NSM-1 and the NSM-2 districts.

**16.20.030.4.1. Neighborhood Suburban Multifamily-1 (NSM-1).**

This district allows multifamily structures. Additional density is possible when workforce housing **or Missing Middle housing** is provided. Building heights typically range between one and three stories.

**16.20.030.5. Maximum development potential.**

Development potential is slightly different within the districts to respect the character of the neighborhoods. Achieving maximum development potential will depend upon market forces, such as minimum desirable unit size, and development standards, such as minimum lot size, parking requirements, height restrictions, and building setbacks.

Minimum Lot Area, Maximum Density and Maximum Intensity

		NSM-1	NSM-2
Minimum lot area (square ft.)		4,500	4,500
Maximum residential density (units per acre)	Residential density	15	24
	<b>Missing Middle housing density bonus</b>	<b>15</b>	<b>N/A</b>
	Workforce housing density bonus	6	6
Maximum nonresidential intensity (floor area ratio)		0.50	0.60
Maximum impervious intensity (site area ratio)		0.65	0.75
<p>Workforce housing bonus: All units associated with this bonus shall be utilized in the creation of workforce housing units as prescribed in the City's workforce housing program and shall meet all requirements of the program.</p> <p><b>A Missing Middle housing density bonus is allowed for multi-family uses at a maximum density of 30 dwelling units/acre and following all dimensional and design requirements in Section 16.20.015 Neighborhood Traditional Mixed Residential-1 (NTM-1) and NTM parking requirements. The Missing Middle housing density bonus is not allowed in addition to the Workforce housing density bonus.</b></p> <p>Refer to technical standards regarding measurement of lot dimensions, calculation of maximum residential density, nonresidential floor area and impervious surface.</p> <p>For mixed use developments, refer to additional regulations within the use specific development standards section for mixed uses (currently section 16.50.200).</p>			

**16.20.060. CORRIDOR RESIDENTIAL TRADITIONAL DISTRICTS (CRT)**

**16.20.060.4. Introduction to CRT districts.**

The CRT districts are the CRT-1 and the CRT-2 districts.

**16.20.060.4.1. Corridor Residential Tradition-1 (CRT-1).**

This district allows multifamily structures. Additional density is possible when workforce housing **or Missing Middle housing** is provided. Building heights typically range between one and three stories.

**16.20.060.5. Development potential.**

Development potential is slightly different within the districts to respect the character of the neighborhoods. Achieving maximum development potential will depend upon market forces, such as minimum desirable unit size, and development standards, such as minimum lot size, parking requirements, height restrictions, and building setbacks.

Minimum Lot Size, Maximum Density and Maximum Intensity

		<b>CRT-1</b>	<b>CRT-2</b>
Minimum lot area (square ft.)		4,500	4,500
Maximum residential density (units per acre)	Residential density	24	40
	Residential density within activity center	60	60
	Workforce housing density bonus	8	6
	<b>Missing Middle housing density bonus</b>	<b>6</b>	<b>N/A</b>
	<b>Missing Middle housing density bonus within activity center</b>	<b>N/A</b>	<b>N/A</b>
Maximum nonresidential intensity (floor area ratio)	Nonresidential intensity	1	1.5
	Nonresidential intensity within activity center	2.5	2.5
	Workforce housing intensity bonus	0.2	0.2
Maximum impervious surface (site area ratio)		0.75	0.95

Workforce housing density and intensity bonus: All units associated with this bonus shall be utilized in the creation of workforce housing units as prescribed in the City's workforce housing program and shall meet all requirements of the program.

**A Missing Middle housing density bonus is allowed for multi-family use at a maximum density of 30 dwelling units/acre following all dimensional and design requirements in Section 16.20.015 Neighborhood Traditional Mixed Residential-1 (NTM-1) and NTM parking requirements. The Missing Middle bonus is not allowed in addition to the workforce housing bonus.**

Refer to technical standards regarding measurement of lot dimensions, calculation of maximum residential density, nonresidential floor area and impervious surface.

For mixed use developments, refer to additional regulations within the use specific development standards section for mixed uses (currently section 16.50.200).

A 100% intensity bonus is allowed for manufacturing, office, and laboratories and research and development uses on parcels designated as Target Employment Center (TEC) Overlay on the future land use map.



**16.20.080. CORRIDOR COMMERCIAL TRADITIONAL DISTRICTS (CCT)**

**16.20.080.4. Introduction to CCT districts.**

The CCT districts are the CCT-1 and the CCT-2 districts.

**16.20.080.4.1. Corridor Commercial Traditional-1 (CCT-1).**

This district generally allows one-story to three-story development containing mixed uses with multifamily structures. Additional density is possible when affordable workforce housing or Missing Middle housing is provided.

**16.20.080.5. Development potential.**

Development potential is slightly different within the districts to respect the character of the neighborhoods. Achieving maximum development potential will depend upon market forces, such as minimum desirable unit size, and development standards, such as minimum lot size, parking requirements, height restrictions, and building setbacks.

Minimum Lot Size, Maximum Density and Maximum Intensity

		CCT-1	CCT-2
Minimum lot area (square ft.)		4,500	4,500
Maximum residential density (units per acre)	Residential density	24	40
	Residential density within activity center	36	60
	Missing Middle Housing density bonus	6	N/A
	Missing Middle Housing density bonus within activity center	N/A	N/A
	Workforce housing density bonus	8	6
	Hotel density (rooms per acre)	45	45
	Hotel density (rooms per acre) within activity center	80	80
Maximum nonresidential intensity (floor area ratio)	Nonresidential intensity	1.0	1.5
	Nonresidential intensity within activity center	1.5	2.5
	Workforce housing intensity bonus	0.2	0.2
Maximum impervious surface (site area ratio)		0.95	0.95

Workforce housing density and intensity bonus: All units associated with this bonus shall be utilized in the creation of workforce housing units as prescribed in the City's workforce housing program and shall meet all requirements of the program.

A Missing Middle housing density bonus is allowed for multi-family use at a maximum density of 30 dwelling units/acre following all dimensional and design requirements in Section 16.20.015 Neighborhood Traditional Mixed Residential-1 (NTM-1) and NTM parking requirements. The Missing Middle bonus is not allowed in addition to the Workforce housing density bonus.

Refer to technical standards regarding measurement of lot dimensions, calculation of maximum residential density, nonresidential floor area and impervious surface.

For mixed use developments, refer to additional regulations within the use specific development standards section for mixed uses (currently section 16.50.200).

**16.20.090. CORRIDOR COMMERCIAL SUBURBAN DISTRICTS (CCS)**

**16.20.090.4. Introduction to CCS districts.**

The CCS districts are the CCS-1 and CCS-2.

**16.20.090.4.1. Corridor Commercial Suburban-1 (CCS-1).**

It is the purpose of this district to generally allow one-story to four-story development containing mixed uses of local interest in conjunction with residential, multifamily units or structures. Additional building height and density is possible within primary and secondary activity centers. Additional density is possible when workforce housing **or Missing Middle housing** is provided.

**16.20.090.5. Development potential.**

Development potential is slightly different within the districts to respect the character of the neighborhoods. Achieving maximum development potential will depend upon market forces, such as minimum desirable unit size, and development standards, such as minimum lot size, parking requirements, height restrictions, and building setbacks.

Minimum Lot Size, Maximum Density and Maximum Intensity

		CCS-1	CCS-2
Minimum lot width	Small lot (less than 1.0 acre)	100 ft.	100 ft.
	Medium lot (between 1.0 - 2.0 acres)	200 ft.	200 ft.
	Large lot (greater than 2.0 acres)	300 ft.	300 ft.
Minimum lot area (square ft.)		4,500	4,500
Maximum residential density (units per acre)	Residential density	15	40
	Residential density within activity center	60	60
	<b>Missing Middle Housing density bonus</b>	<b>15</b>	<b>N/A</b>
	<b>Missing Middle Housing density bonus within activity center</b>	<b>N/A</b>	<b>N/A</b>
	Workforce housing density bonus	8	10
	Workforce housing density bonus within activity center	10	15
	Hotel density (rooms per acre)	45	55
	TDR density bonus	9	0
Maximum nonresidential intensity (floor area ratio)	Nonresidential intensity	0.55	0.75
	Nonresidential intensity within activity center	2.5	1.12
	Workforce housing intensity bonus	0.2	0.2

	Workforce housing intensity bonus within activity center	0.2	0.5
	TDR intensity bonus	0.2	0
Maximum impervious surface (site area ratio)		0.85	0.9
<p>Workforce housing density and intensity bonus: All units associated with this bonus shall be utilized in the creation of Workforce Housing units as prescribed in the City's workforce housing program and shall meet all requirements of the program.</p> <p><b>A Missing Middle housing density bonus is allowed for multi-family use at a maximum density of 30 dwelling units/acre following all dimensional and design requirements in Section 16.20.015 Neighborhood Traditional Mixed Residential-1 (NTM-1) and NTM parking requirements. The Missing Middle bonus is not allowed in addition to the Workforce housing density bonus.</b></p> <p>Hotel density: Additional hotel density may be allowed pursuant to the cg (commercial general) Comprehensive Plan future land use category and section 4.2.7.6 of the countywide plan rules. In order to preserve existing commercial floor area on redevelopment sites within CCS-1 equal to or greater than 5 acres, the residential component shall not exceed 40 percent of the total FAR. Where the residential component exceeds 40 percent of the total FAR, special exception approval is required. Refer to technical standards regarding measurement of lot dimensions, calculation of maximum residential density, nonresidential floor area, and impervious surface. For mixed use developments, refer to additional regulations within the use specific development standards section for mixed uses (currently section 16.50.200). A 100% intensity bonus is allowed for manufacturing, office, and laboratories and research and development uses on parcels designated as Target Employment Center (TEC) Overlay on the future land use map.</p>			

**SECTION 16.90.020. - RULES OF INTERPRETATION AND DEFINITIONS**

**16.90.020.3. – Definitions**

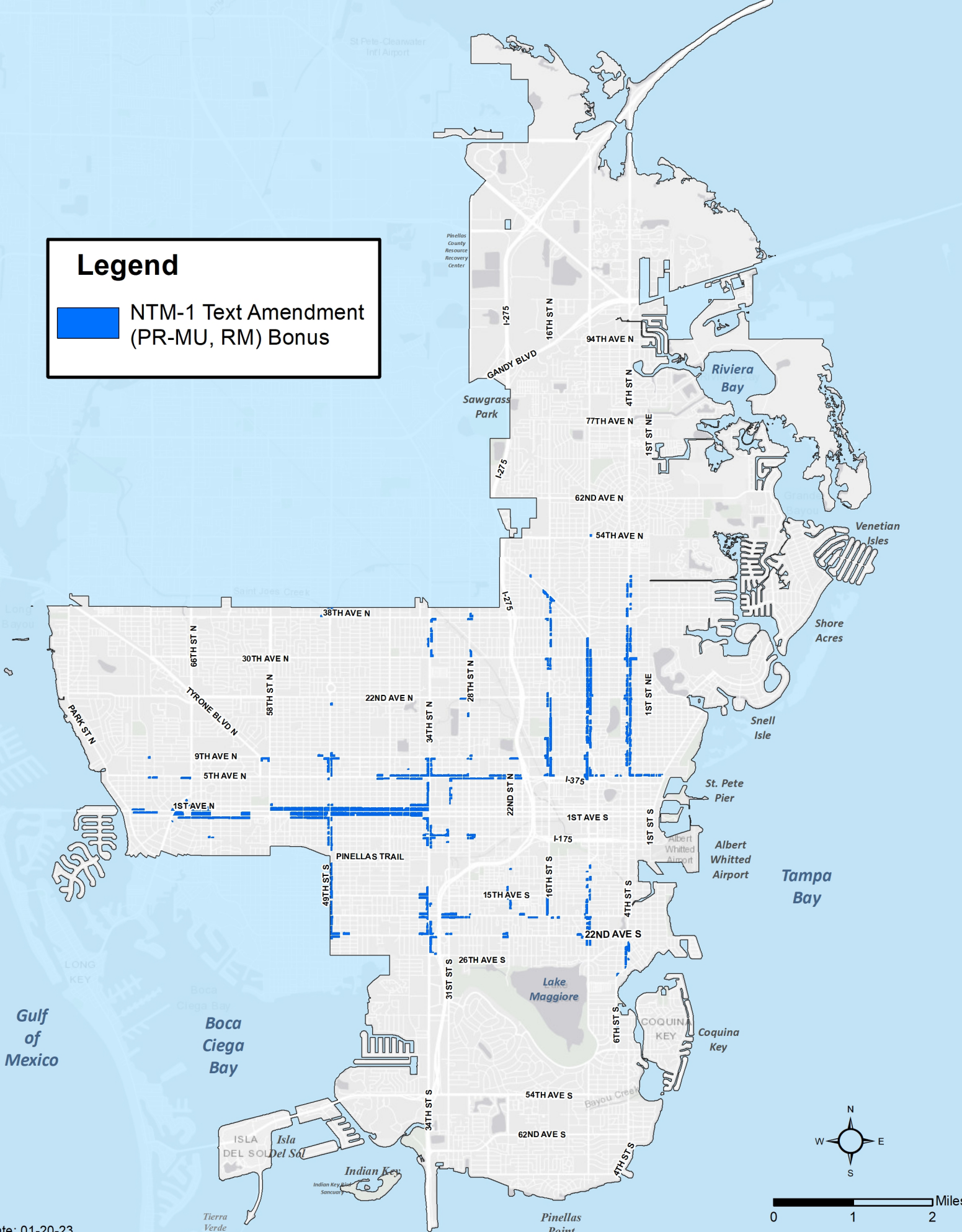
**Missing Middle Housing** encompasses a range of smaller, multi-unit or clustered housing types (such as shotgun, skinny, duplex, triplex, fourplex, courtyard apartment, bungalow court, townhouse, multiplex, and live/work units), which are compatible in scale and design with single-family homes, and are designed to encourage walking, biking, and transit use.

**Maps of NSM-1, CRT-1, CCT-1 and CCS-1 Properties**

# Legend



NTM-1 Text Amendment  
(PR-MU, RM) Bonus

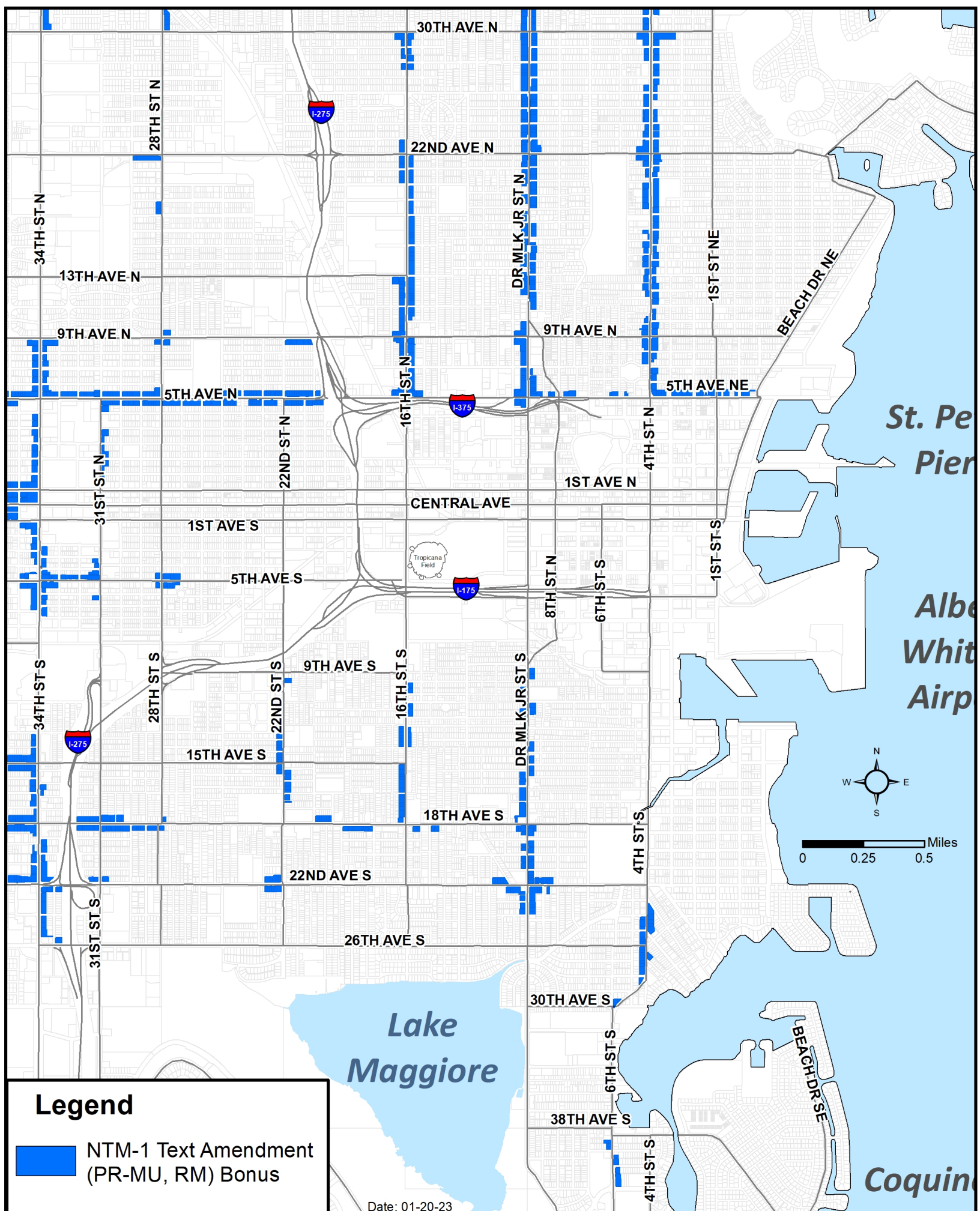


Date: 01-20-23

Planning and Development Services  
City of St. Petersburg

## NTM-1 (Neighborhood Traditional Mixed Residential) Zoning






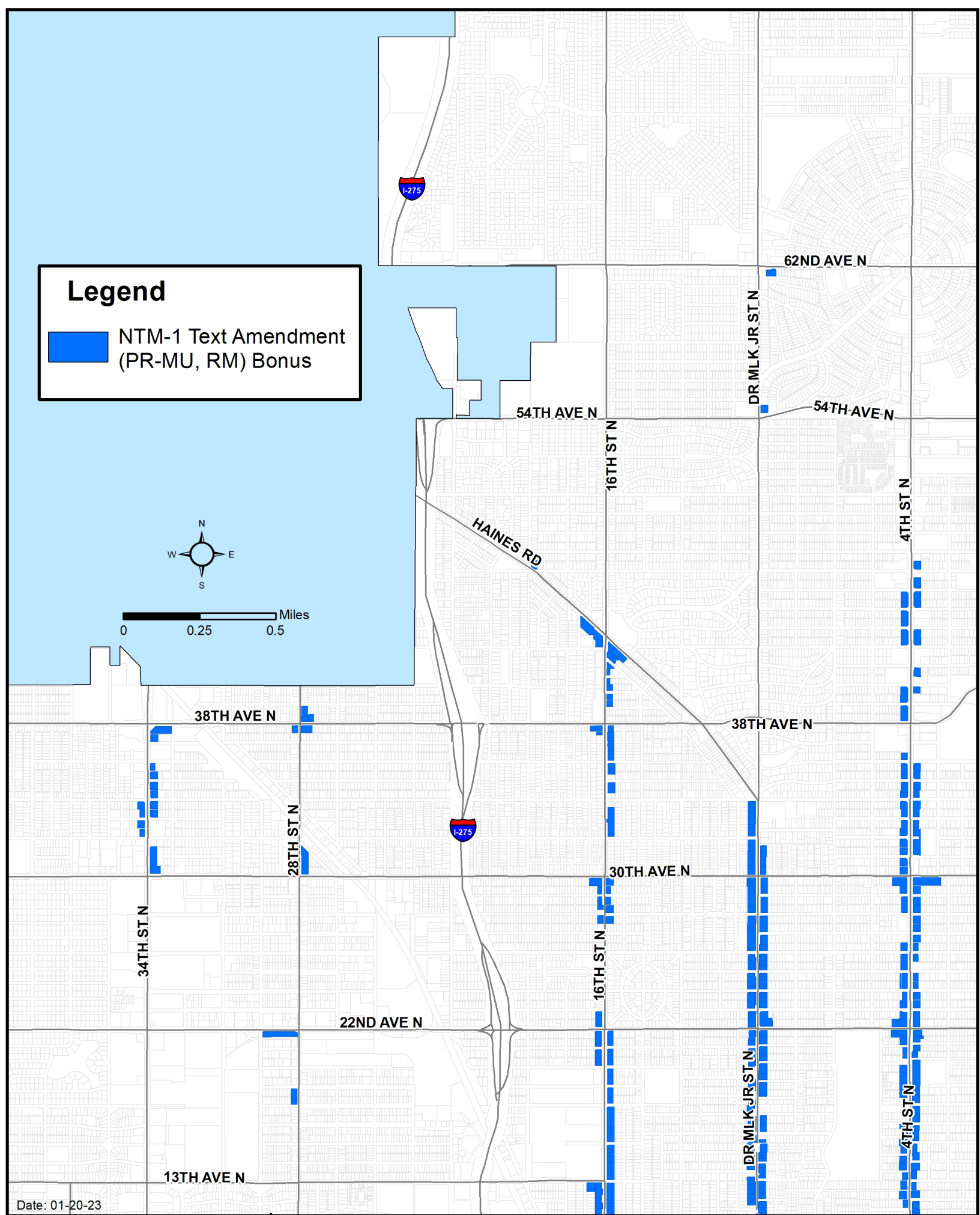
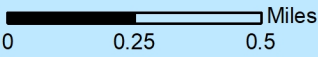
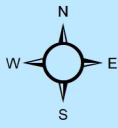
**Legend**

NTM-1 Text Amendment (PR-MU, RM) Bonus

Date: 01-20-23

**Legend**

 NTM-1 Text Amendment (PR-MU, RM) Bonus



Date: 01-20-23







# ATTACHMENTS

## Public Comments

Kate M Thorpe-Eddleman  
President, Palmetto Park Neighborhood Association  
thorpekatem@gmail.com  
(786) 553-9881

December 8, 2022

Dear City of St. Petersburg,

- Request to Maintain Front Parking and Access in Qualified NTM-1 Rezoning Home / Former Church: 2643 5th Ave South, St. Petersburg, Florida, Palmetto Park Neighborhood Association
- Show support NTM-1 Rezoning

As a devoted resident and President of the Palmetto Park Neighborhood Association, I am constantly seeking ways to improve the quality of life in our neighborhood and surrounding areas. I praise the city's plans to expand NTM-1 zoning to alleviate the housing crisis facing our community.

I am writing this letter of support for a unique scenario concerning a Palmetto Park Neighborhood Association home/former church. The NTM-1 rezoning proposal currently states that all parking must be in the back alley. However, this property, originally a small church and now a residential home, was built significantly further back on the lot to also accommodate front parking. Therefore, while the property fully qualifies for the NTM-1 rezoning, it may require the removal of existing front parking and access.

**Given the unique nature of the property, I am asking for the NTM-1 zoning proposal to allow the current owner to maintain the front parking and access, retaining her property's qualification of the proposed rezoning.** Allowing this will remove a significant obstacle for the owner and facilitate the creation of additional residential units on the property.

I am confident that you will grant this request, recognizing that it will encourage the modification of a unique, and important structure in the Palmetto Park Neighborhood, while allowing the community to benefit from additional dwelling units through NTM-1's rezoning.

I look forward to your decision.

Sincerely,

*Kate M Thorpe Eddleman*

December 12, 2022

Kate M Thorpe-Eddleman  
President, Palmetto Park Neighborhood Association

December 9, 2022

Gina Marie Foti  
2643 5th Ave South, St. Petersburg, Florida, 33712  
[Imaginationten@gmail.com](mailto:Imaginationten@gmail.com) | (727) 902-1252

Dear Community Planning and Preservation Commission (CPPC) and City of St. Petersburg,

**NTM-1 Property:** 2643 5th Ave South, St. Petersburg, Florida, 33712

**Re:** Request to Maintain Front Parking and Access in Former Church / Home in Qualified NTM-1 Rezoning, as Part of Ongoing Efforts to Mitigate St. Petersburg's Housing Crisis

I am pleased to say that my home qualifies and fits all the requirements for the NTM-1\* rezoning.

Although I fully support and commend the City for these efforts, my home presents a unique scenario that requires thoughtful consideration. My property was previously a small neighborhood church with cultural significance, and was built and designed substantially further back on the lot than the surrounding homes to allow front access parking (see Exhibit A).

As it stands, the NTM-1 proposal states that all required parking spaces must be accessed exclusively from the alley. Therefore, if I take advantage of the proposed NTM-1 zoning, it may require the removal of my home's existing front parking and access.

### **Request**

Given the unique nature of my home, formerly a church, I am asking for the NTM-1 zoning to be amended to allow the property to maintain its front parking access.

### **Benefits of Approving Zoning Request**

If my request is approved, it will provide the following benefits to the community:

- Encourage the modification of an existing property, while complementing the structure, its unique nature and cultural neighborhood past.
- Allow a St. Petersburg resident to participate in the NTM-1 rezoning program.
- Promote walkability, "at the core of the success of increasing walkability is density, it is the key to making these communities walkable and vibrant." Urban Land Institute\*\*
- Prevent burdensome, costly and unknown variances.
- Benefit the community with additional dwelling units, and therefore support NTM-1's primary objective.

On the other hand, if I am required to remove the front parking, or have to go through unknown variances, it would put me at a great disadvantage, and would encourage the removal of the existing structure, creating an unnecessary, expensive, and time-consuming burden, significantly delaying the potential for additional units that can become available to the community.

### **Conclusion**

While I appreciate the parking parameters in the NMT-1 rezoning for most case scenarios, I urge you to allow my request to amend the NTM-1 rezoning and allow my property to maintain its front parking access. If we work together, we can develop and sharpen many wonderful tools to solve the housing crisis in St. Petersburg.

The NTM-1 proposed change aims to increase housing supply, and to "enable various dwelling units in

response to market demands, encouraging the use and preservation of current structures.” I purchased a former church and not a regular single-family home as housing shortages have us all exploring creative ways to address the situation.

### **Community Support**

Attached is a letter from the President of the Palmetto Park Neighborhood Association, supporting retaining the parking and front access of the property due to its unique nature (See Exhibit B).

Also attached, is a letter from Bishop Zema Florence. prior owner, and operator of the community church, supporting my request. As you will read in his letter, it is of importance to him to have part of this structure be included in the new proposed NTM-1 zoning, and feels strongly that allowing the front parking and entrance to remain on the site will facilitate additional units to the community (See Exhibit C).

I appreciate your consideration.

Sincerely,

*Gina Marie Foti*

### **Gina Marie Foti**

Homeowner  
2643 5th Ave South  
St. Petersburg, Florida

\*Under this proposed change, single-family houses may expand to include accessory dwelling units (e.g. garage apartments) or be redeveloped up to a maximum four (4) residential units. These units may be developed as rental apartments, townhouses, or condominiums.) Source: City of St. Petersburg website ([https://www.stpete.org/residents/current\\_projects/planning\\_projects/housing\\_initiatives.php](https://www.stpete.org/residents/current_projects/planning_projects/housing_initiatives.php))

\*\* The Benefits of Growth - Urban Land Institute. 1 Mar. 2019, [https://uli.org/wp-content/uploads/ULI-Documents/BenefitsofGrowth.ashx\\_.pdf](https://uli.org/wp-content/uploads/ULI-Documents/BenefitsofGrowth.ashx_.pdf).

### **Exhibit A:**



**See Support Letters in the Following Pages**

Kate M Thorpe-Eddleman  
President, Palmetto Park Neighborhood Association  
thorpekatem@gmail.com  
(786) 553-9881

December 8, 2022

Dear City of St. Petersburg,

- Request to Maintain Front Parking and Access in Qualified NTM-1 Rezoning Home / Former Church: 2643 5th Ave South, St. Petersburg, Florida, Palmetto Park Neighborhood Association
- Show support NTM-1 Rezoning

As a devoted resident and President of the Palmetto Park Neighborhood Association, I am constantly seeking ways to improve the quality of life in our neighborhood and surrounding areas. I praise the city's plans to expand NTM-1 zoning to alleviate the housing crisis facing our community.

I am writing this letter of support for a unique scenario concerning a Palmetto Park Neighborhood Association home/former church. The NTM-1 rezoning proposal currently states that all parking must be in the back alley. However, this property, originally a small church and now a residential home, was built significantly further back on the lot to also accommodate front parking. Therefore, while the property fully qualifies for the NTM-1 rezoning, it may require the removal of existing front parking and access.

**Given the unique nature of the property, I am asking for the NTM-1 zoning proposal to allow the current owner to maintain the front parking and access, retaining her property's qualification of the proposed rezoning.** Allowing this will remove a significant obstacle for the owner and facilitate the creation of additional residential units on the property.

I am confident that you will grant this request, recognizing that it will encourage the modification of a unique, and important structure in the Palmetto Park Neighborhood, while allowing the community to benefit from additional dwelling units through NTM-1's rezoning.

I look forward to your decision.

Sincerely,

*Kate M Thorpe Eddleman*

December 12, 2022

Kate M Thorpe-Eddleman  
President, Palmetto Park Neighborhood Association

Bishop Zema J. Florence  
The Holy Temple of Church  
(727) 452-3191 | florencezema@gmail.com

December 9, 2022

**Re:** Request to Maintain Front Parking and Access in Former Church / Home for Qualified NTM-1 Property

**Subject Property:** Qualified NTM-1 Rezoning Property, 2643 5<sup>th</sup> Ave South, St. Petersburg, Florida, 33712

To Whom It May Concern:

This letter is regarding my former property and church (now a residence), 2643 5<sup>th</sup> Ave South, St Pete, Florida 33712, which I am pleased to say qualifies for the NTM-1 rezoning proposal.

As the previous owner and operator of the church, formerly known as the Holy Temple Cathedral Church of God in Christ, it is important to me that the property continues to benefit the community which it served for years, but now as additional housing.

Since the property was built as a small church, it was constructed further back on the lot to accommodate for front access and parking. While the property fully qualifies for the NTM-1 rezoning, it may require the removal of the existing front parking and access. **Given the unique nature of the property, I am asking for the NTM-1 zoning proposal to allow the current owner to maintain the front parking access.**

Allowing the front parking and entrance will make it easier for the home to be modified to accommodate additional units. My concern is that if the front parking is required to be removed, it will create costly obstacles to the new owner and may incentivize not using any of the current structure.

It was a difficult decision to sell the church, but we secured a larger location to accommodate our growing congregation. I sold Ms. Foti the property, because she cares about the community, knowing it would be in good hands.

Prior to my time as Bishop, the church had played a key role in St. Petersburg, as part of the Living God Worldwide Revival Center. Its founding father, Pastor Samuel Butler, an influential gospel musician and his sons, recorded with the Five Blind Boys of Alabama, one of the groups which helped end segregation in the United States.

During my time at the church, we served hundreds of members with holiday celebrations and gatherings, food drives, and clothes giveaways, welcoming key members of our community to participate in our services, including the late Reverend Watson Haynes, III as one of our keystone Black History Month speakers.

Modifying the property to provide housing will be an incredible part of its journey, allowing it to continue to serve its community. I urge you to allow the home to retain its front parking and entrance, so that the current owner does not have unnecessary hardships in order to benefit from NTM-1.

Sincerely,



Bishop Zema J Florence

December 12, 2022

Bishop Zema J. Florence  
The Holy Temple of Church  
(727) 452-3191 | florencezema@gmail.com

December 9, 2022

**Re:** Request to Maintain Front Parking and Access in Former Church / Home for Qualified NTM-1 Property

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Sincerely,



Bishop Zema J Florence

December 12, 2022

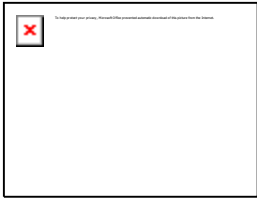
## Ann O. Vickstrom

---

**From:** Dustin Baldwin <Dustin@velocityvg.com>  
**Sent:** Friday, January 20, 2023 11:15 AM  
**To:** Ann O. Vickstrom  
**Subject:** Re: NTM Zoning Question

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Thank you for the information, very helpful! Have a great weekend Ann.



**Dustin Baldwin**  
Founder/CEO at VVG

**M** [727.421.8228](tel:727.421.8228)

**E** [dustin@velocityvg.com](mailto:dustin@velocityvg.com)



On Jan 20, 2023, at 10:56, Ann O. Vickstrom <Ann.Vickstrom@stpete.org> wrote:

Dustin,

Thank you. The property is currently zoned CRT-1 and while we are not proposing to rezone that property to NTM-1, we are proposing a text amendment to allow any CRT-1 property an option of using a "Missing Middle Housing" bonus of up to 6 units/acre if following all NTM-1 locational, and design parameters. That would allow the same 30 units/acre as the NTM-1 zoning designation. No other housing bonuses are allowed. This is proposed for final public hearing on March 23, so we will have to wait on the outcome of City Council vote. This is an optional allowance, otherwise, a developer can follow the CRT-1 requirements. However, I have included the NTM-1 district requirements for your information.

Respectfully,

Ann Vickstrom, AICP, RLA#0001122  
Planner II, Urban Planning and Historic Preservation  
City of St. Petersburg  
(727)892-5807  
[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)

Please note all emails are subject to public records law.



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**From:** Dustin Baldwin <Dustin@velocityvg.com>  
**Sent:** Friday, January 20, 2023 9:55 AM  
**To:** Ann O. Vickstrom <Ann.Vickstrom@stpete.org>  
**Subject:** Re: NTM Zoning Question

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good morning Ann, it doesn't have an address yet, 2525 4th St S will get you there via Google Maps, or if you look up Blue Door Building Co via property appraiser it will bring up the 4th St S parcel number.

<image001.jpg>



**Dustin Baldwin**  
Founder/CEO at VVG

---

**M** [727.421.8228](tel:727.421.8228)

**E** [dustin@velocityvg.com](mailto:dustin@velocityvg.com)

On Jan 20, 2023, at 08:22, Ann O. Vickstrom <[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)> wrote:

Dustin,

Can you give me an address for this property?

Thank you,

Ann Vickstrom, AICP, RLA#0001122  
Planner II, Urban Planning and Historic Preservation  
City of St. Petersburg  
(727)892-5807  
[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)

Please note all emails are subject to public records law.

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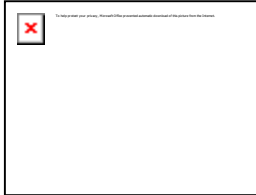
**From:** Dustin Baldwin <[Dustin@velocityvg.com](mailto:Dustin@velocityvg.com)>  
**Sent:** Wednesday, January 18, 2023 4:51 PM  
**To:** Ann O. Vickstrom <[Ann.Vickstrom@stpete.org](mailto:Ann.Vickstrom@stpete.org)>  
**Subject:** NTM Zoning Question

**CAUTION:** This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Mrs. Vickstrom,

I have a lot that I am in planing and design on for 3 units (Triplex) in the CRT-1 zoning. I just noticed (after getting the postcard) that it is in the proposed NTM zoning. Does this mean I can develop 4 units here now? The parcel is under Blue Door Building CO LLC on 4th St S.

Thank you for the assistance!



**Dustin Baldwin**  
Founder/CEO at VVG

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**M** [727.421.8228](tel:727.421.8228)

**E** [dustin@velocityvg.com](mailto:dustin@velocityvg.com)

<SECTION\_16.20.015.\_\_\_\_NEIGHBORHOOD\_TRADITIONAL\_MIXED\_RESIDENTIAL\_DISTRICTS\_\_\_\_NTM\_\_\_\_  
(3).docx>

**Current NTM-1 District**

## **SECTION 16.20.015. NEIGHBORHOOD TRADITIONAL MIXED RESIDENTIAL DISTRICTS ("NTM")**

### **16.20.015.1. Purpose.**

To provide a variety of urban housing choices in low to medium density building types that reinforce the walkability of the neighborhood, provide a variety of attainable housing choices, establish appropriate transition zones from mixed-use corridors to single-family housing, support neighborhood-serving retail and service uses adjacent to this zoning category, and support public transportation and other multi-modal alternatives.

Development standards reinforce the traditional development pattern. Street standards preserve the alley system as a mechanism for providing limited access to parking and utility functions in the rear of the site.

(Ord. No. 405-H, § 4, 12-12-2019)

### **16.20.015.2. Applicability.**

Uses in this district shall be allowed as provided in the Matrix: Use Permissions and Parking Requirements.

- A. Applicable to locations that transition from a mixed-use corridor, center or Future Major Street to a single-family neighborhood. The most effective application of this district is in a linear configuration when located within 175-feet of the centerline of a designated *Future Major Street* or *High Frequency Transit Route* with service head-way times equal to, or less than, 35-minutes.
1. Qualified properties shall be adjacent to a public alley.
  2. Applicable to traditional neighborhoods, where the subject property:
    - a. Retains direct connectivity to one or more adjoining *Future Major Streets* or *High-Frequency Transit Routes*; and
    - b. Is located outside of the designated Coastal High Hazard Area ("CHHA").
  3. Where listed in the St. Petersburg Register of Historic Places as an individual local landmark or contributing resource to a local historic district, or where listed in the National Register of Historic Places as an individual listing or contributing resource to a historic district, new dwelling units above the existing number of dwelling units shall only be allowed when adaptively established within the existing principal structure. Additions and accessory buildings may include new dwelling units when designed subordinate to the principal structure and in accordance with the applicable review procedures.

(Ord. No. 405-H, § 4, 12-12-2019)

### **16.20.015.3. Introduction to the NTM-1 district.**

The standards for the NTM-1 district are intended to allow for renovations and redevelopment within the traditional neighborhoods, while respecting the existing development pattern and unique character of these areas.

This district will allow for a variety of building typologies. These building typologies, commonly referred to as "Missing Middle" housing types, reinforce urban, walkable neighborhoods with a combination of single-family and multi-family residential units located near daily destinations. These units provide attainable life-cycle housing to a diverse group of residents including first-time homeowners, families, couples, retirees, adults with disabilities, and car-free households.

This district will allow density up to 30-units per acre, not to exceed four dwelling units per building. Accessory dwelling units, such as garage apartments, are allowed, subject to compliance with density standards, building setbacks, parking and other applicable requirements. The design guidelines are intended to ensure compatibility with the existing character and pattern of these neighborhoods by requiring compatible building design and driveways, garages, and utility uses are limited to the rear of the property.

(Ord. No. 405-H, § 4, 12-12-2019)

**16.20.015.4. Maximum development potential.**

Achieving maximum development potential will depend upon market forces, such as minimum desirable unit size, and development standards, such as minimum lot size, parking requirements, height restrictions, floor area ratios, maximum building and impervious surface ratios, and building setbacks.

**TABLE 16.20.015.4.a: Minimum Lot Standards and Lot Coverage**

<b>Lot Standards</b>	
Lot Area, Minimum: Residential	1,452 square feet per unit
Lot Area, Minimum: Non-Residential	22,860 square feet
Lot Width, Minimum: Residential	20 feet
Lot Width, Minimum: Non-Residential	150 feet
<b>Lot Coverage</b>	
Impervious Surface, Maximum: Residential	0.75 or 75%
Impervious Surface, Maximum: Non-Residential	0.65 or 65%
Building Coverage, Maximum <sup>1</sup> : Residential	0.60 or 60%
<sup>1</sup> Includes all enclosed structures	

Preservation of neighborhood character is critical to any successful renovation or redevelopment. For this reason, floor area ratio ("FAR") standards are applied to new construction. Design standards may be incorporated to increase the maximum FAR, where such design standards help achieve compatibility between the proposed renovations and redevelopment with neighboring houses.

**TABLE 16.20.015.4.b: Maximum Density and Maximum Intensity**

<b>Density</b>	
Density, Maximum <sup>1</sup> : Residential	30 units per acre
<sup>1</sup> Includes accessory dwelling unit(s)	
<b>Intensity<sup>1,2,3</sup></b>	
Intensity, Maximum: Residential	0.50 FAR
Intensity, Maximum: Non-Residential	0.50 FAR
<sup>1</sup> Maximum intensity does not include FAR bonuses, which are calculated separately.	

<sup>2</sup> Includes any enclosed space <i>above</i> the required design flood elevation line; excludes that portion of the enclosed space that is <i>below</i> the required design flood elevation line.	
<sup>3</sup> Does not include the first 200 square feet of enclosed garage per unit.	
<b>FAR Bonuses</b>	
Bonus, Maximum: Residential	0.20 FAR
<i>The following options may be incorporated in any combination, not to exceed the maximum bonus allowed - 0.20 FAR:</i>	
a. One story covered front porch with a separate roof structure with a minimum width of 90 percent of the front façade. No bonus is allowed if there is a second story deck, porch or roof structure.	0.08
b. Additional second story front setbacks: .01 bonus for every 1-foot of additional front setback of the entire façade, and .005 bonus for every 1-foot of additional front setback of at least one-third of the façade but which is less than the entire façade, no bonus is allowed unless the setback is at least 6-feet, maximum 0.10 bonus. No bonus is allowed if there is a second story deck, porch or roof structure.	Variable, 0.10 max
c. Additional second story side setbacks: .01 bonus for every 1-foot of additional side setback of the entire façade, maximum 0.05 bonus per side.	Variable, 0.05 max per side
d. Total residential floor area of the second story does not exceed 75 percent of the first story (excludes garage SF).	0.05
e. The entire peak of the primary roof structure of the front façade is parallel to the front property line: bonus 0.02, or if the entire peak of the primary roof structure of the front façade is parallel to the front property line and the roof has dormer(s) which are equal to at least 20 percent of the width of the front façade: 0.04 bonus.	Variable, 0.04 max
f. Side façade articulation: side façades that feature offsets of at least 2-feet in depth that are at least 12-feet in length that divide the building design and are in the front two thirds of the side façade: 0.02 bonus per side, maximum 0.04.	Variable, 0.04 max
g. Front façade articulation: front façades (excluding the porch) which feature offsets of at least 6-feet in depth for a minimum of one third of the front façade, 0.06 bonus for each additional foot, maximum 0.10.	Variable, 0.10 max
h. Certified LEED or Florida Green Building	0.05
i. Solar ready	0.02
<b>Additional Notes:</b>	
Refer to technical standards regarding measurement of lot dimensions, calculation of maximum residential density, non-residential floor area and impervious surface.	
For mixed use developments, refer to additional regulations within the use specific development standards in the Mixed Uses Section.	

(Ord. No. 405-H, § 4, 12-12-2019)

**16.20.015.5. Building envelope: height, setback, and width.**

**TABLE 16.20.015.5.a: Maximum Building Height**

<b>Building Height<sup>1</sup></b>	
<b>Principal Structure</b>	
Beginning of Roofline	24-feet
Top of roof peak	36-feet
<b>Accessory Structure(s)</b>	
Beginning of Roofline	20-feet
Top of roof peak	30-feet
<sup>1</sup> Refer to technical standards regarding measurement of building height and height encroachments.	

**TABLE 16.20.015.5.b: Minimum Building Setbacks**

<b>Building Setbacks<sup>1,2,3</sup></b>	
Front: Steps Extending from Porch or Stoop	8-feet or M
Front: Porch or Stoop	12-feet or M
Front: Building	18-feet or M
Side, Interior	3-feet or M
Side, Street	8-feet or M
Rear, Alley	22-feet, including width of alley
<b>Special Exception</b>	
All yards	25-feet
<sup>1</sup> M (minor encroachment): Minor encroachments into normally prescribed setbacks may be allowed in order to accommodate an addition to align with the side of the existing structure, provided: (a) The total floor area of the encroaching portion of an addition shall not exceed 50 square feet; (b) No portion of the encroachment shall exceed 24 feet in height.	
<sup>2</sup> Refer to technical standards regarding measurement of building setbacks and setback encroachments.	
<sup>3</sup> The larger of the minimum building separation distances required by the Florida Building Code or the Life Safety Code or the minimum building setback established for the interior side yard setback shall apply.	

**TABLE 16.20.015.5.c: Maximum Building Width**

Accessory Dwelling Unit (ADU)	Not applicable
Residential	40-feet maximum
Non-Residential	Not applicable

(Ord. No. 405-H, § 4, 12-12-2019)

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**16.20.015.6. Setbacks and FAR consistent with established neighborhood patterns.**

There are building setback and FAR characteristics of existing neighborhoods related to front yard setbacks, FAR, and alignment of buildings along the block face. Minimum yard setback and FAR characteristics of neighborhoods may differ from the requirements of this district. The POD may approve, without a variance, residential development that meets these setback and FAR characteristics. Approval shall be based on the following:

1. Front yard setbacks will be based on predominant building setbacks established in the block in which the development is proposed.
2. FAR will be based on predominant building FAR established in the block in which the development is proposed based on the Property Appraiser's Records.
3. Predominant shall mean equal to or greater than 50%.
4. These are administrative approvals appealable only by the property owner.

(Ord. No. 405-H, § 4, 12-12-2019)

**16.20.015.7. Entrances.**

The number and location of entrances can have a consequential impact on the compatibility of multi-family housing with surrounding single-family housing. These standards are intended to reinforce the residential character of the surrounding neighborhoods.

**TABLE 16.20.015.7: Entrances<sup>1</sup>**

Accessory Dwelling Unit ("ADU")	Per ADU standards
Detached House	1 entrance facing the primary street
Duplex	1 entrance minimum, 2 entrances maximum, facing the primary street; on corner lots, each unit entrance shall face a different street, except where entrances are provided from within an interior vestibule or hallway.
Triplex and Fourplex	1 entrance minimum, 2 entrances maximum, facing the primary street; on corner lots, each unit entrance shall face a different street, except where entrances are provided from within an interior vestibule or hallway.
Bungalow Court	Each main entrance shall face the shared court. Cottages abutting the primary street shall have their main entrance facing the primary street.
<sup>1</sup> Pedestrian connections shall link each exterior entrance to the public rights-of-way, private open space, and parking areas.	

(Ord. No. 405-H, § 4, 12-12-2019)



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### 16.20.015.8. Building and site design.

The following design criteria allow the property owner and design professional to choose their preferred architectural style, building form, scale and massing, while creating a framework for good urban design practices.

*Site layout and orientation.* The City is committed to creating and preserving a network of linkages for pedestrians.

*Building layout and orientation.*

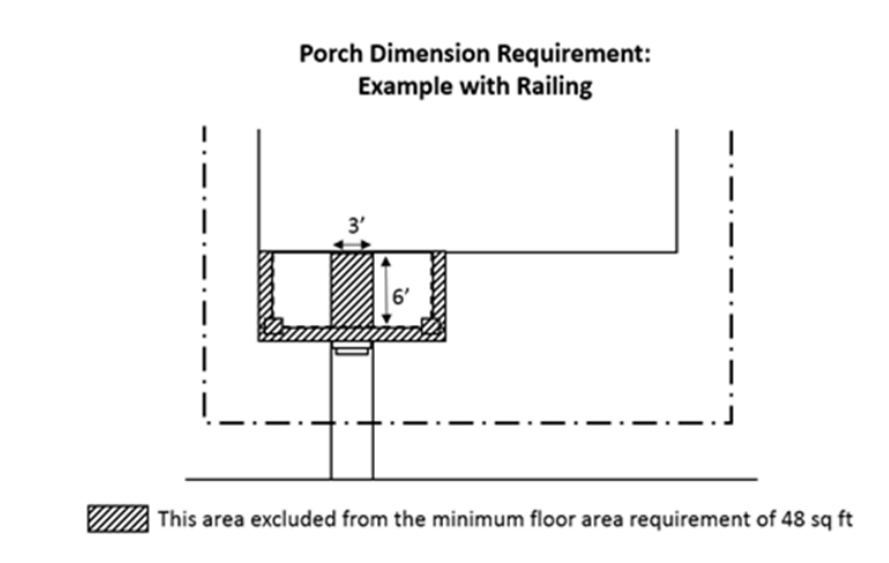
1. For non-residential uses, all service areas and loading docks shall be located behind the front façade line of the principal structure.
2. All mechanical equipment and utility functions (e.g. electrical conduits, meters and HVAC equipment) shall be located behind the front façade line of the principal structure. Mechanical equipment that is visible from the primary street shall be screened with a material that is compatible or consistent with the architecture of the principal structure.
3. Accessory structures (including sheds) shall be located behind the front façade line of the principal structure.

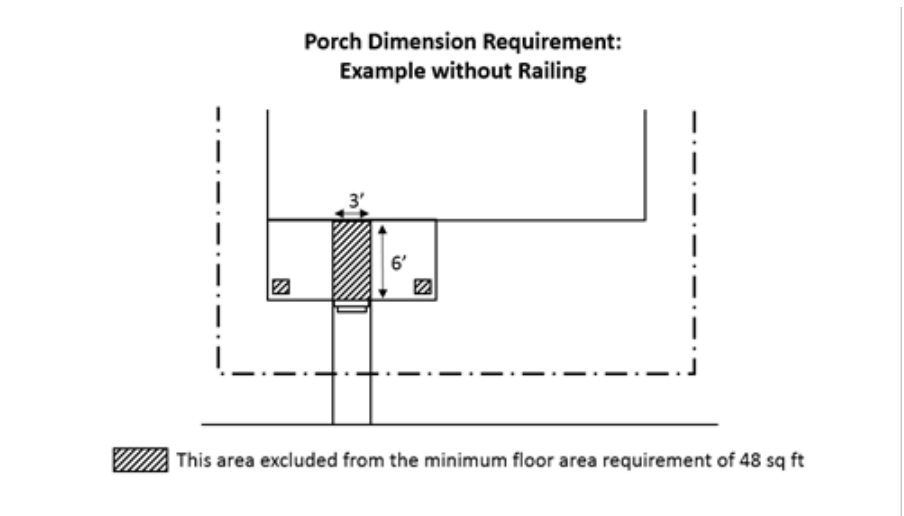
*Vehicle connections and parking.*

1. All parking shall be accessed from an alley.
2. Garage doors shall face the alley.
3. All parking spaces shall be located behind the plane of the front building face.

*Porches and pedestrian connections.*

1. Principal entries shall include a porch, with a minimum usable depth of 6-feet (measured from the front façade line of the structure to the interior side of the railing or, if there is no railing, the furthest edge of the floor) and 48 square feet of total floor area, excluding a three-foot wide walkway to the primary entrance and the floor area of any column. Where a railing exists, only the floor area within the interior side of the railing shall count towards the minimum floor area.
2. Existing public sidewalks shall be repaired to City standards. Where no public sidewalk exists, a public sidewalk shall be constructed in accordance with the requirements of the subdivision section.



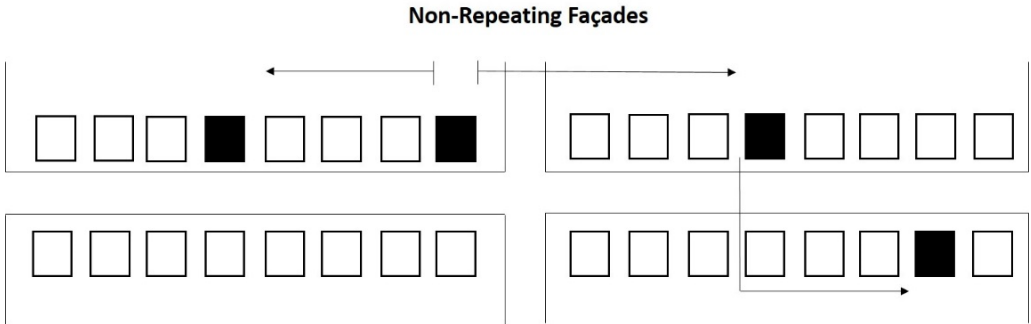


*Building and architectural design standards.* All buildings should present an inviting, human scale façade to the streets, internal drives, parking areas and surrounding neighborhoods. The architectural elements of a building should give it character, richness and visual interest.

*Building style.*

1. New construction shall utilize an identifiable architectural style which is recognized by design professionals as having a basis in academic architectural design philosophies. See the Architecture and Building Design Section.
2. Design of buildings on the same block face on either side of the street or within an adjacent block face on either side of the street shall be varied, such that a substantially similar design will not be replicated. Bungalow courts are exempt from this non-repeat standard. Other creative layouts involving multiple buildings on a single parcel may be approved at the discretion of the POD.

There shall be a minimum separation of three parcels in every direction before a substantially similar design can be repeated. Variation shall include at least three of the following elements: architectural style, roof form (principal or porch), materials, or architectural details (e.g., doors, windows, columns, porches).



3. Renovations, additions and accessory structures shall utilize the architectural style of the existing structure, or the entire existing structure shall be modified to utilize an identifiable architectural style which is recognized by design professionals as having a basis in academic architectural design philosophies.

*Building form.*

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1. The front porch shall be elevated at least 12 inches above the abutting finished grade level as measured abutting the porch at the front entry.
  2. The front façade of a building shall create a width-to-height ratio of no more than 1:1. Buildings that exceed the width-to-height ratio of 1:1 shall feature architectural fenestration creating a bay system that divides the building design into a maximum ratio of 1:1. This may be done through pilasters, arcades, building line and roof line off-sets, materials and other appropriate architectural features.

*Wall composition and transparency.* Wall composition standards ensure that ground-level storefronts and multifamily and single-family residential buildings offer attractive features to the pedestrian. Wall composition standards also mitigate blank walls and ensure that all sides of a building have visual interest. Transparency enhances visual connections between activities inside and outside buildings, thereby improving pedestrian safety. The following criteria shall not apply to accessory structures.

1. Doors, windows and other appropriate fenestration, architectural details and features shall be incorporated into all sides of a building. There shall be no blank façades, except that garages located at the rear one-third of the lot may have blank façades but not on the street side. No portion of a façade shall contain a blank area greater than 16 feet in width.
2. At least 30 percent of primary and secondary street façades shall consist of fenestration or architectural details and features. At least 20 percent of the front two-thirds of interior side façades shall consist of fenestration or architectural details and features. At least ten percent of the rear façade on corner lots and through lots shall consist of fenestration or architectural details and features. At least 50 percent of the required fenestration shall be transparent (i.e., window glass). For yards on through-lots see the Dimensional Regulations and Lot Characteristics Section.
3. Structures which are situated on corner lots, through lots, or, by the nature of the site layout have a façade which is clearly visible from rights-of-way, shall be designed with full architectural treatment on all sides visible from rights-of-way. Full architectural treatment shall include roof design, wall materials, trim, and door and window openings. While it is recognized that buildings have primary and secondary façades, the construction materials and detailing should be similar throughout. Windows on the street side façades shall be evenly distributed in a consistent pattern, unless a different proportion is permitted or required by an identifiable architectural style.
4. Window sashes and glass shall be square or vertical, unless a different proportion is permitted or required by an identifiable architectural style.
5. Windows shall not be flush mounted. Windows recessed less than three inches shall feature architectural trim including a header, sill and side trim or decorative shutters. Windows recessed three inches or more shall feature a window sill. Trim is not required if not consistent with the architectural style, i.e. Modern or Mediterranean Revival.
6. Where the required design elevation is equal to or greater than 48 inches above finished grade, an articulated base is required to delineate the first-floor level. The base may consist of a different material or decorative band, depending on the architectural style.

*Roofs.* Rooflines add visual interest to the streetscape and establish a sense of continuity between adjacent buildings. When used properly, rooflines can help distinguish between residential and commercial land uses, reduce the mass of large structures, emphasize entrances, and provide shade and shelter for pedestrians. Buildings shall provide a pitched roof or a flat roof with a decorative parapet wall compatible with the architectural style of the building.

*Building materials.* Building material standards protect neighboring properties by holding the building's value longer, thereby creating a greater resale value and stabilizing the value of neighboring properties. Building materials shall be appropriate to the selected architectural style and shall be consistent throughout the structure except for one story covered patios or screen enclosures located at least ten feet behind the front façade of the

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principal structure. If multiple materials are used in a building façade, the visually heavier materials shall be located below the lighter materials, e.g. brick or stone shall be located below stucco or siding materials, unless they are used as architectural features.

*Accessory structures and ancillary equipment and carports.* Accessory structures shall reinforce the pedestrian character of the City. Above-ground utility and service features, accessory storage structures, and carports shall be located and designed to reduce their visual impact upon the streetscape. See use specific standards in the Accessory Structures and Ancillary Equipment Section. Detached accessory structures, such as garages and garage apartments shall be consistent with the architectural style, materials, and color of the principal structure. For multi-story buildings, no portion of an exterior wall on any floor may contain a blank area greater than 16-feet in width except as allowed herein for garages.

(Ord. No. 405-H, § 4, 12-12-2019)